

Martin Luther King, Jr. Boulevard

Streetscape and Urban Design Master Plan

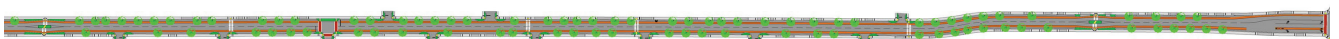
*Created by the Community of Oak Park
Final Administrative Draft, June 9, 2008*



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1. Introduction

Oak Park is one of Sacramento's oldest communities with a remarkable palette of history, opportunities, and challenges. This Master Plan envisions the revitalization of Martin Luther King, Jr. Boulevard and the neighborhoods of South Oak Park that it serves. This document is the result of a sixteen-month collaborative process that has involved residents, the City of Sacramento, the Sacramento Housing and Redevelopment Agency, and other institutions. Participants in this process are united in their goal of creating a sound, integrated strategy for the future - one that improves the quality of life and enhances the character of the boulevard. This Master Plan describes the beginning of an ongoing process of revitalization. It is consistent with the guiding principles of Sacramento's General Plan and it serves as a starting point for future improvement strategies and future development proposals.

The goals of this Master Plan are:

- Improve the quality of life for all residents
- Enhance neighborhood character
- Improve circulation networks for the safety of drivers, bicyclists, and pedestrians
- Improve infrastructure for the neighborhood
- Carefully consider plans for growth and development that is consistent with existing development patterns which decrease in intensity as one travels south along the corridor
- Celebrate the history of Martin Luther King, Jr. and the culture of the neighborhood
- Improve communication between all members of the community, city and neighboring institutions

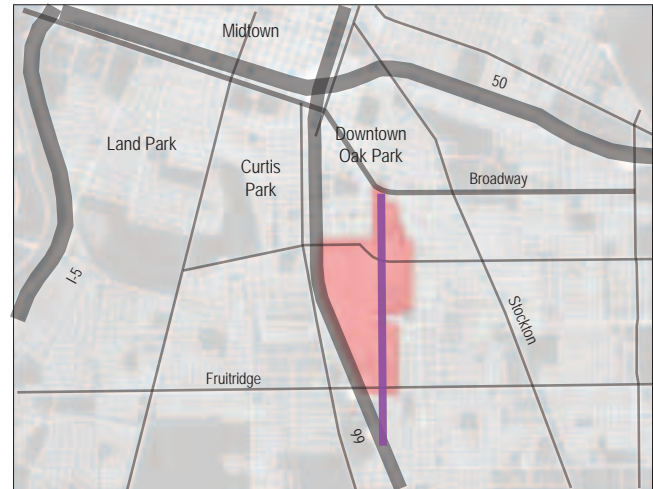


Project Area

This Master Plan covers the area of southern Oak Park shown in the adjacent map. This area is within the City of Sacramento and also within the Oak Park Redevelopment Area. The area shown is approximately 430 acres of urbanized land. This area includes residential neighborhoods, commercial areas, and numerous schools.

Martin Luther King, Jr. Boulevard has been identified as the focus of Streetscape and Urban Design improvements because it is the primary spine of activity and its current condition does not honor the cultural importance of Dr. Martin Luther King, Jr., its namesake. The safe circulation of traffic, bicycles, and pedestrians through the neighborhood is a prime area of concern.

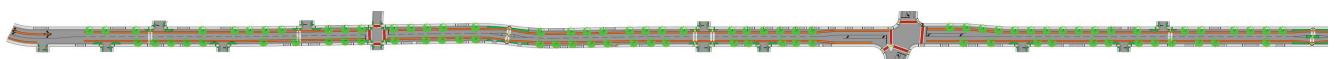
The neighborhoods on either side of the boulevard are also included within this project scope. These are mostly single-family residential areas. Census data from 2000 indicates that approximately 7,000 residents live on blocks within or immediately adjacent to the study area. The infrastructure needs of these residents are addressed in this project, along with access to parks, libraries and infill development opportunities.



City Context of Project Area and Martin Luther King, Jr. Blvd.



Aerial View of Project Area



Project Description

This Master Plan will help drive future changes in the southern Oak Park neighborhood while clearly stating guidelines that respect its traditions and history. In scope, this plan:

- Describes public process
- Describes the existing conditions within the community and current challenges in detail
- Details proposed traffic calming, infrastructure, architectural and landscape improvements
- Offers economic analysis of potential infill opportunity sites
- Details resources and options for funding infrastructure and development over time



Participants voice their priorities for the neighborhood plan

A Neighborhood Plan

Neighborhood planning is an opportunity for citizens to shape the neighborhoods where they live, work, own property or manage a business. The neighborhood planning process addresses land use, zoning, transportation, urban design and related neighborhood issues. The goal of neighborhood planning is for diverse interests to come together and develop a shared vision for their community.

A neighborhood plan represents the views of all stakeholders that make up a community. It should:

- Identify neighborhood strengths and assets.
- Identify neighborhood needs and concerns.
- Establish goals for improvements
- Presents specific recommendations to reach those goals.
- Provide a framework for other decisions.

- Give direction to the City and the Housing & Redevelopment Agency about appropriate capital improvements for the neighborhood.
- Inform property owners and potential developers about what businesses are needed and the types of development that should be encouraged.

Fiscal Impact & Long-term Benefits

The fiscal impact of this Master Plan cannot be overstated; the cost of potential streetscape improvements for Martin Luther King, Jr. Boulevard alone is currently estimated at \$5.9 million. The value of the investment in the South Oak Park neighborhood over time however will yield far more than simply return on money invested; tangible improvements will build positive cultural, social, and community-building momentum.



Project Process

The Sacramento Housing and Redevelopment Agency (SHRA), in partnership with the City of Sacramento, the Oak Park Community and others initiated a planning process to guide future land use, development, infrastructure for southern Oak Park and streetscape design for Martin Luther King, Jr. Boulevard.

SHRA sponsored a neighborhood improvement workshop between March 14th and March 21, 2007. Workshop planning was done with the Councilmember and the Project Management Team (PMT). The PMT is composed of City and SHRA Staff, the Oak Park Neighborhood Association, Sacramento Regional Transit and the consultant team.

The event was very well attended. This may have been due to the fact that postcards describing the workshops were mailed to property owners and residents in the planning area. Additionally, news releases were sent to local media. Posters and flyers were also distributed throughout the neighborhood and banners publicizing the workshop were placed in strategic locations. Unfortunately, few members from the southern portion of Oak Park attended.

For purposes of this report, a design workshop is similar to a design charrette. A design workshop is a multi-day series of meetings, presentations and workshops that engage citizens and local staff in a variety of interactive activities designed to elicit their concerns and suggestions, provide information about possible solutions and foster cooperative development of a community vision. Each of the design workshop's activities is summarized in Appendix 1.



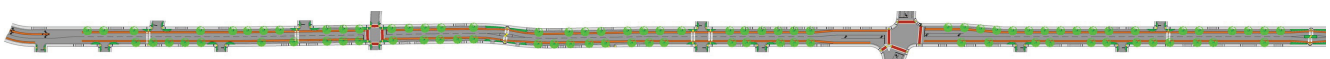
Public Workshops were publicized in many ways



Workshop participants observe MLK Jr. Blvd.



Community members put their ideas to paper

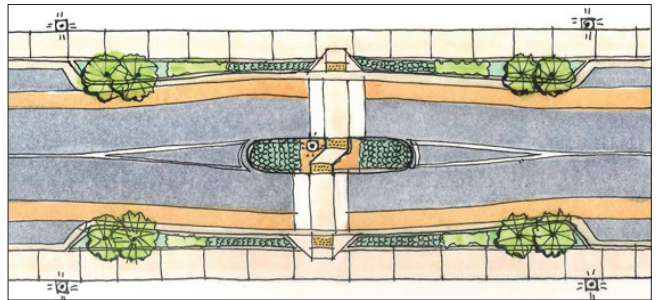


Project Highlights

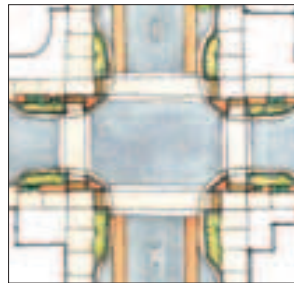
- Redesign of Martin Luther King, Jr. Boulevard
- Traffic Calming Measures
- Infrastructure Improvements
- Landscape & Park Improvements
- Infill Development Concept Plans
- Public Art Opportunities



Chapter 3: Proposed Streetscape Improvements



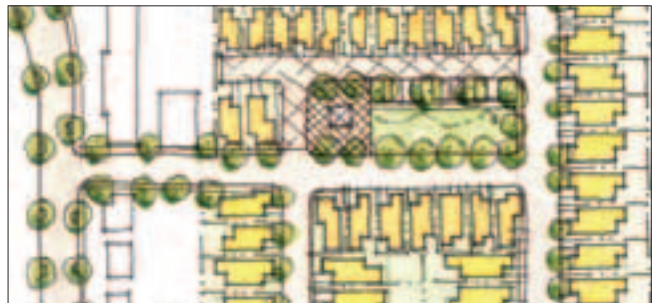
Chapter 3: Pedestrian Oasis (crossing refuge)



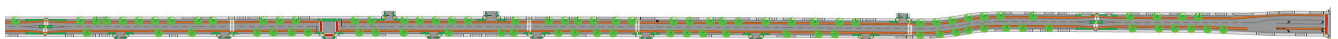
Chapter 3: Bulbouts

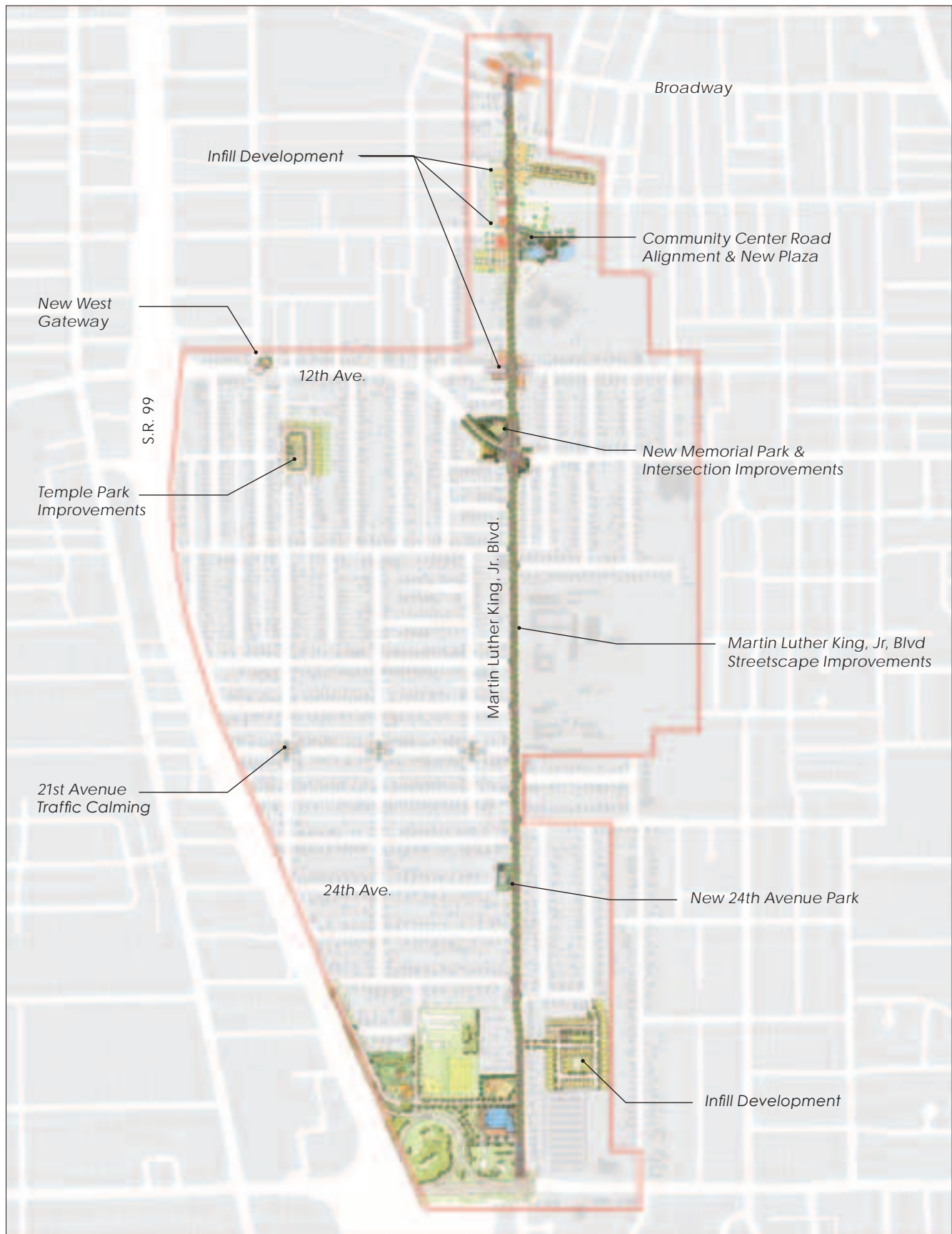


Chapter 4: New park design

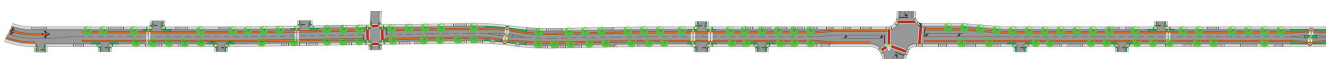


Chapter 5: Infill Development Concept Plan





Project Map highlighting potential improvements

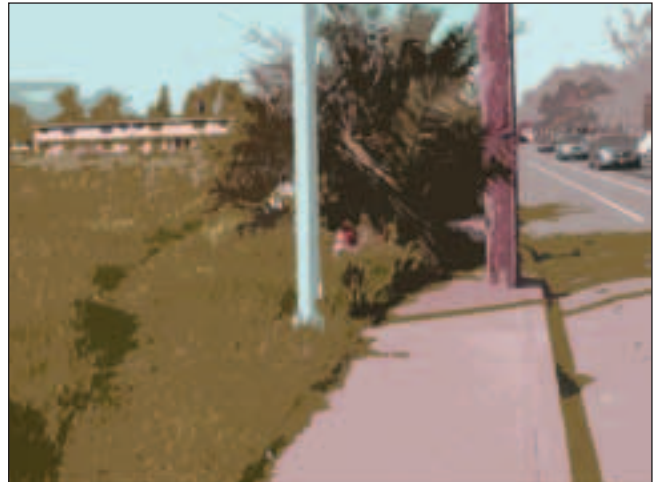


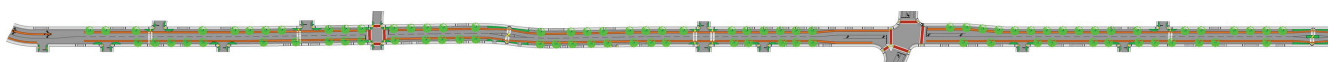
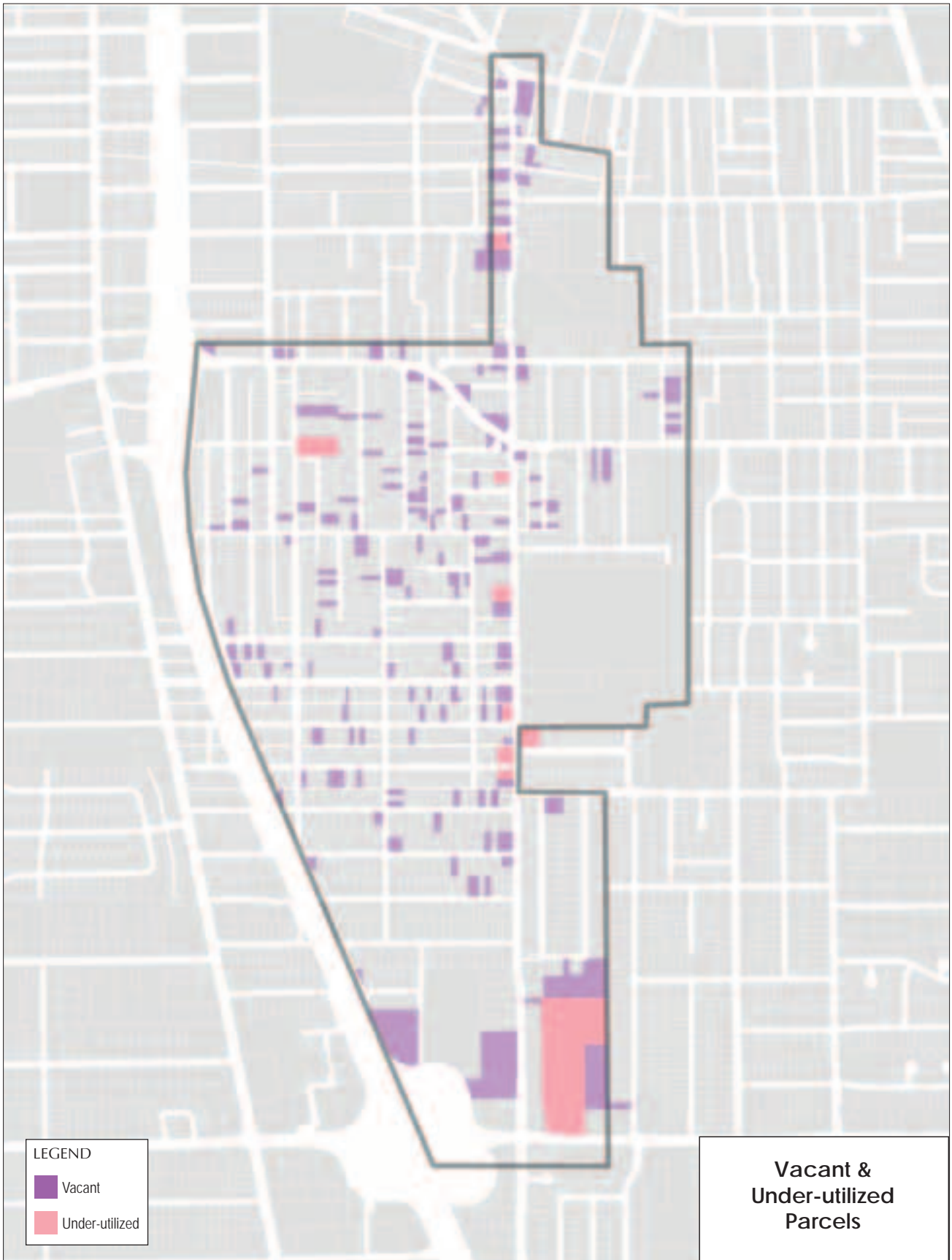
2. Existing Conditions

This section describes the existing context of the neighborhood, with careful analysis of road and transit networks, parks, schools, land uses, opportunity sites, and infrastructure elements. The analysis of the study area was performed using multiple sources of information. These included site observation and measurement, City-provided GIS data, City-provided infrastructure data, previously completed reports, and meetings with project stakeholders.

Topics covered in this chapter are:

- Vacant & Underutilized Parcels
- Commercial Land Uses
- Schools
- Parks
- Roadway networks
- Existing Alleys
- Transit Networks
- Overhead Utilities
- Underground Utilities





Vacant & Under-utilized Parcels

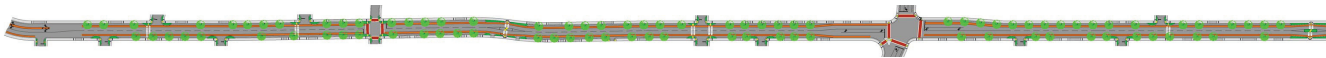
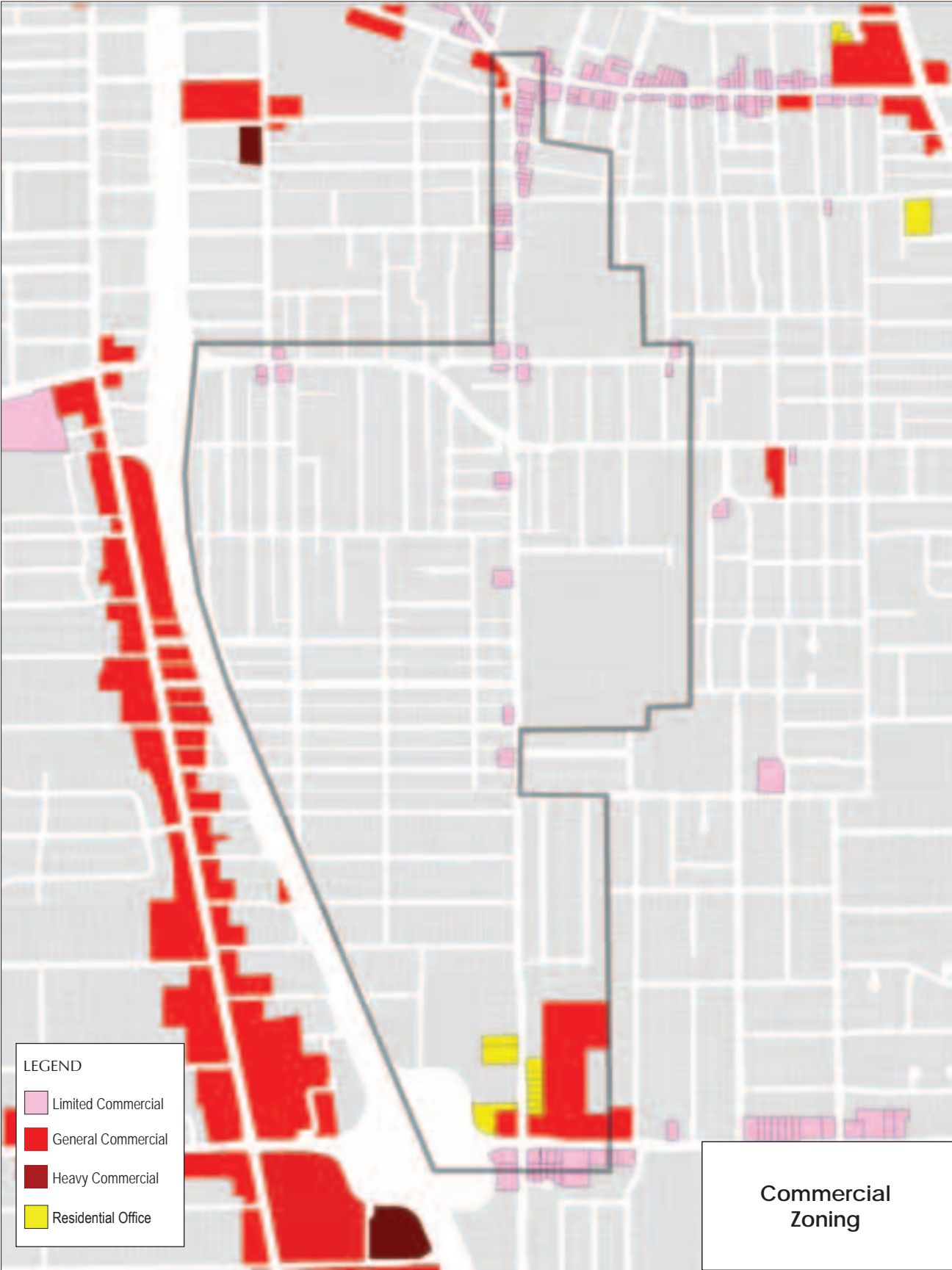
Vacant and under-utilized (not used to its highest potential from a land use or density/intensity perspective) parcels are found throughout the study area. Generally speaking, most of these are isolated single family parcels and do not offer significant opportunities for change. In addition, many parcels are being developed with new housing. The current housing market has brought this development to a near standstill, but we may assume that this trend will continue in the long term.



Vacant Parcels at Martin Luther King, Jr. Blvd. & 12th Ave.

A few larger parcels do stand out, however, as significant opportunity sites. At the southeast of the study area, close to Fruitridge Road, vacant land sits behind single family homes and a public storage facility. At the southwest, a large vacant area is comprised of undeveloped school grounds and two large undeveloped parcels. At 12th Avenue and Martin Luther King, Jr. Boulevard are four parcels, one on each corner. And across the boulevard from the Oak Park Community Center are a combination of vacant parcels and underutilized commercial properties that could combine to create a more vibrant "heart" of the southern Oak Park community.





Commercial Zoning and Land Uses

The study area contains commercially zoned parcels along Martin Luther King, Jr. Boulevard and 12th Avenue. As shown on the adjacent exhibit, these parcels are concentrated at the northern and southern ends of Martin Luther King, Jr. Boulevard. Several other commercial parcels are distributed throughout the area but are mostly isolated in their locations. Businesses would be more viable if clustered and at major crossroads, rather than in their current locations.

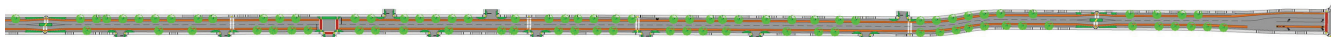


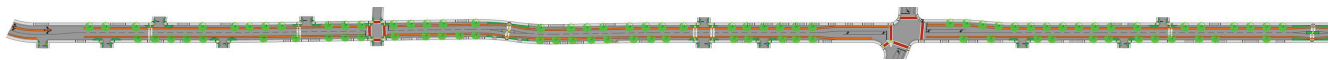
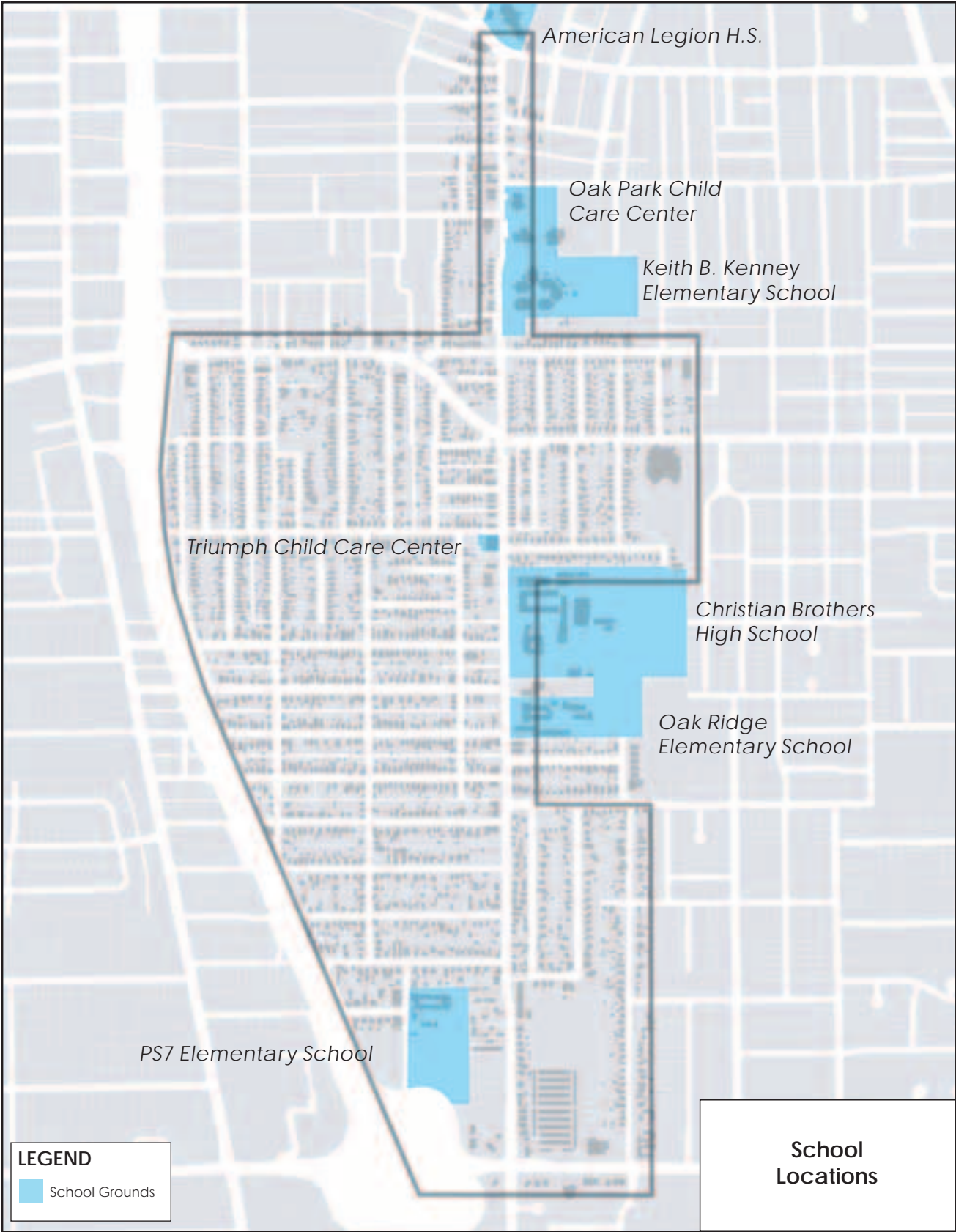
Existing Commercial-zoned parcel

As an example, the four parcels at 12th Avenue and Martin Luther King, Jr. Boulevard are all vacant. They previously occupied a prominent intersection, with through traffic, before 12th Avenue was re-routed to meet 14th Avenue. A commercial use is no longer the most appropriate for the current street layout. Additionally, the large commercial parcel at the southeast corner of the study area contains a large vacant portion surrounded by residences. This commercially-zoned parcel may be more appropriate for residential uses. In either case residential uses are permitted in these commercial zones so there is no need to rezone the properties in order for redevelopment to occur.

While providing local business establishments is a goal of this and any community plan, the existing commercial businesses do not provide for all of the needs of the community. Residents take issue with many of the existing convenience markets; through the community workshops, they indicated a desire for more healthy markets and for additional restaurants where residents, students, and church-goers could walk for a convenient and healthy meal.

The proposals in this plan are consistent with the proposed 2030 General Plan.





Schools

Residents have characterized Martin Luther King, Jr. Boulevard as an “educational corridor.” This reflects the reality that the area is home to several schools, including two preschools, three elementary schools, and two high schools. In addition, McGeorge School of Law is nearby in the historic portion of Oak Park.

Safe access to school sites is a primary goal of this master plan. Current school layouts, schedules, and drop-off arrangements do not provide a safe environment for children; improvements are recommended to provide safer routes to school.

Schools are also important for creating social capital and for providing a sense of community. Participation by schools, for example, in community art projects would be beneficial for creating a sense of ownership by the community's youngest residents.



Keith B. Kenney Elementary School



Oak Ridge Elementary School



American Legion High School



PS-7 Elementary School

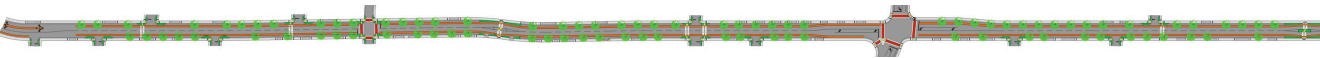


Christian Brothers High School



Triumph Child Care Center





Parks

Oak Park is home to the historic and distinguished McClatchy Park, an asset that provides a desirable open space in the historic areas of the community. Future improvements to this park will enhance this amenity even further. The southern portion of Oak park, however, is characterized by a lack of parks. There are two parks within this study area: the Oak Park Community Center and Temple Park. Each of these parks could be vastly improved to be more accessible, open, and safe. There are no parks south of Temple Park. This area has a large and diverse population of residents who would benefit by the creation of new public parks.

The overall acreage of this project study area is approximately 430 acres. According to year 2000 U.S. Census data, the blocks within or immediately adjacent to the study area are home to approximately 7,000 residents in 1,800 households.

The City of Sacramento Parks and Recreation Master Plan from November, 2005, assesses the need for public parks based upon the number of park acres per 1000 residents. The stated goal of the parks master plan is 5 acres per 1000 residents; of this total, half should be "neighborhood-serving parks" (up to 10 acres in size) and half should be "community-serving parks" (10-60 acres in size).

Given the Park Service Level Goals, and the estimated population of the study area of 7,000 residents, this area should ideally have 35 acres of park area. The two existing park facilities within the study area are the Oak Park Community Center and Temple Park. The Community Center is calculated as providing 7 acres of community-serving park, while Temple Park provides 1 acre of neighborhood-serving park. McClatchy Park, located approximately



Playground at Temple Park

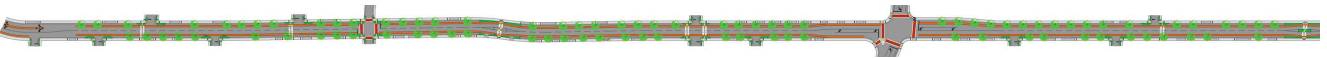
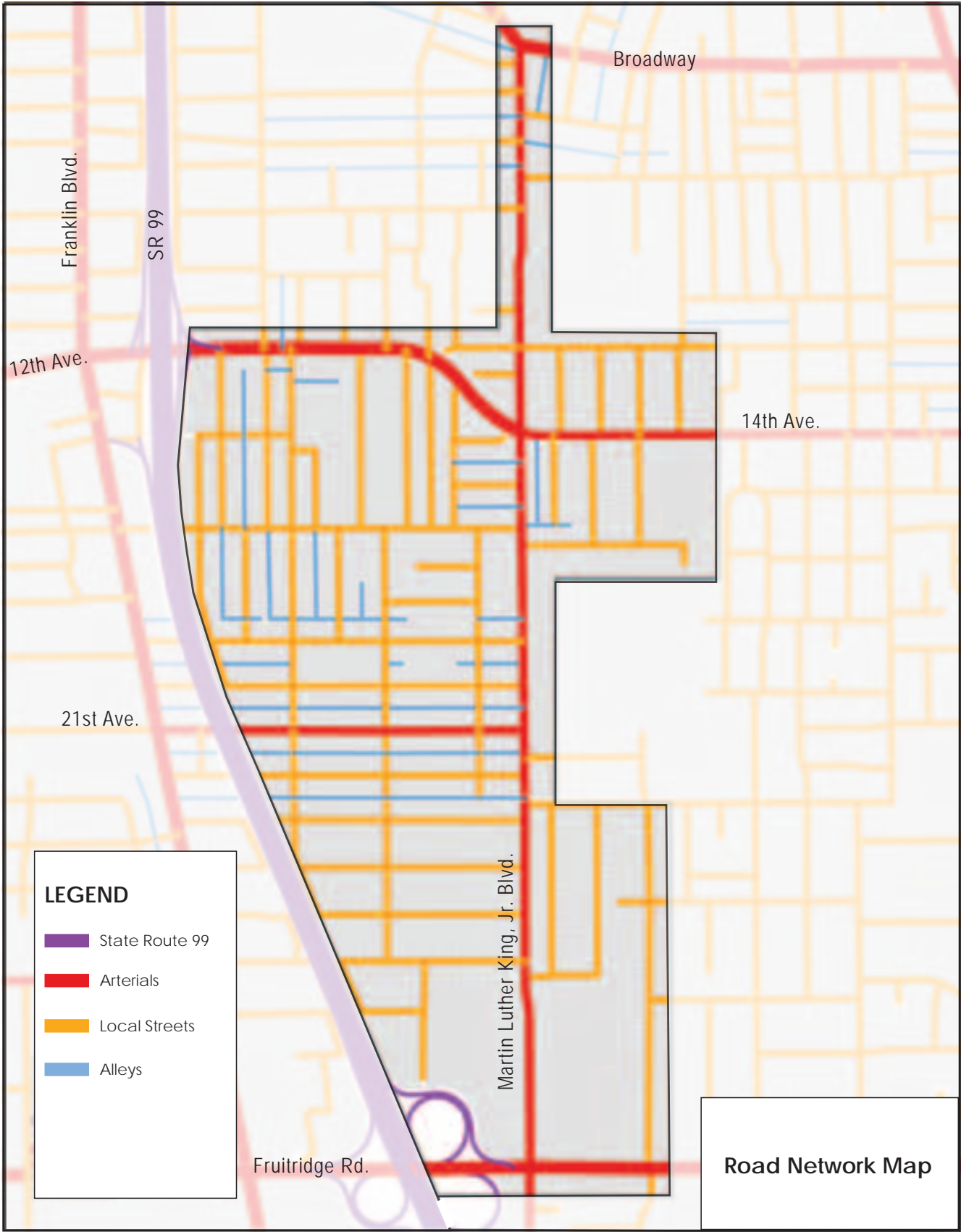
1/2 mile north of the study area, provides 5 acres of neighborhood-serving park and 10 acres of community-serving park.

In addition to public parks, school sites may also contribute to the supply of park area. The area has several schools with open-space acreage. These are often gated and not always accessible to residents, but for the purposes of calculating the overall park deficiency, they are included here. Father Keith B. Kenney Elementary School has 5.58 acres; Oak Ridge Elementary School has 7.75 acres; St. Hope Public School 7 has 7.8 acres; and American Legion High School has 5.19 acres. The parks master plan assumes that 40% of school area can be considered as usable park acreage. In this case, the total acreage is 26 acres, and 40% of the total is 10.5 acres.

The total need for parks is approximately 35 acres. Existing parks total 8 acres, and existing schools add 10.5 acres, for a total of 18.5 acres. The deficiency is estimated at 16.5 acres.

The portion of the study area with the greatest need is the southern area. Temple Park is the closest available park facility and it is located one mile from Fruitridge Road, the southern boundary of the study area.





Road Networks

The study area is served by a hierarchy of different roadway types. To the immediate west is State Route 99, a freeway that bounds Oak Park and limits access to the west. Of the streets within the study area, only Fruitridge Road, 21st Avenue, and 12th/14th Avenue provide east to west connections. At the north of the study area, Broadway provides an east to west connection and an important route into historic Oak Park and downtown Sacramento.

Martin Luther King, Jr. Boulevard is the only continuous north to south route within the study area. It is the spine of activity within South Oak Park. Six schools are located on or within a block of the boulevard, adding to high automobile, bicycle, and pedestrian traffic. It also serves as an alternate route into downtown when traffic on SR 99 is congested.

Martin Luther King, Jr. Boulevard is in need of updating to current pedestrian friendly standards. Sidewalks are five feet wide, often with shrubs or leaning fences encroaching into public areas. Sidewalks exist on both sides of the corridor but accessibility is frequently interrupted by non-ADA compliant obstacles. These include utility poles, heaved sections of sidewalk that present tripping hazards, and driveways that do not meet current slope standards. Most of the pedestrian ramps in the project area do not meet current ADA standards. Access to and from the project area on the west is provided by two pedestrian overcrossings of State Route 99. The approach ramps to these overcrossings are not ADA compliant.

Bicycle mobility is adequately served by the existing roadway configuration, with double-striped 5-foot wide Class II bike lanes. This meets the standards defined by the 2010 Sacramento

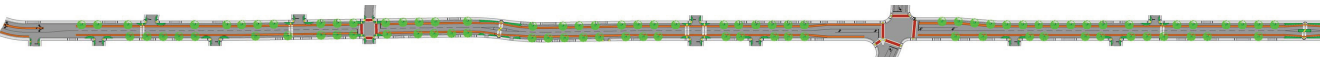
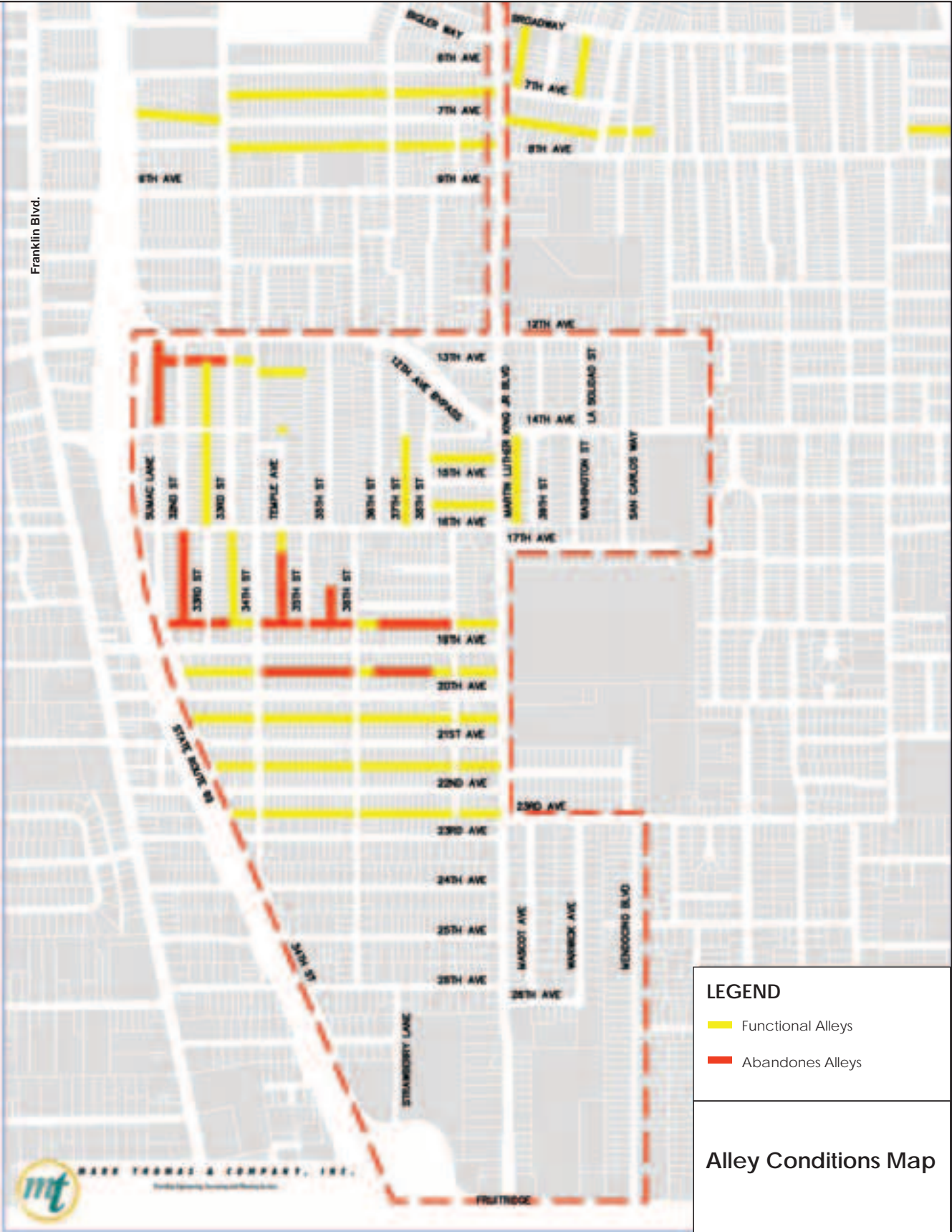


Typical streetscape along Martin Luther King, Jr. Boulevard

City/County Bicycle Master Plan. Because of the high volume of bicycle ridership in the area, any roadway improvements should meet or surpass the existing conditions. One observation made on frequent visits to the project area is that many cyclists ride on the opposite side of the road, facing vehicular traffic. This often occurs where riders feel unsafe and, falsely, believe it is safer to face oncoming traffic.

The width of right-of-way along Martin Luther King, Jr. Boulevard is generally sixty feet. This width varies, but typically includes one 12 to 14-foot lane in each direction with a 12-foot shoulder for on-street parking and bicycle traffic. At the intersection of 12th and 14th Avenues, the right-of-way widens to accommodate turn lanes. Due to the large number of parcels along the corridor, it is assumed that acquiring right-of-way to widen the corridor is not feasible. All of the design solutions shown in this Master Plan fit into the existing right-of-way.





Existing Alleys

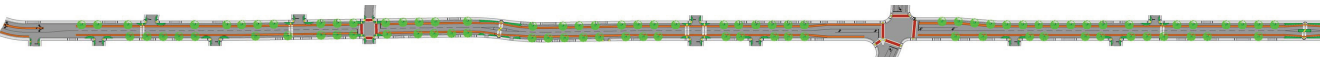
Existing alleys are amenities, opportunities, as well as a potential nuisance and public safety issue. The alleys are unpaved, unlit, and poorly surveilled. In many portions of the project area, especially in the south (south of 16th Avenue), alleys are very long and do not serve many existing buildings.

Many alleys are still an amenity to adjacent residents. They provide a location for trash pickup, for additional access to off-street parking, and to secondary dwelling units. These alleys should be improved to enhance safety.



View of existing alley





Transit Network

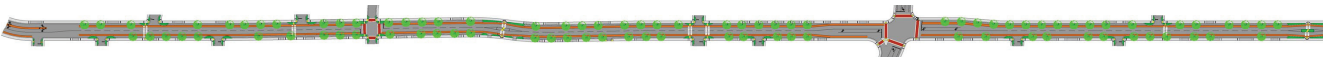
Sacramento's Regional Transit operates four bus routes through this study area. These include the 67 (Franklin), the 68 (44th Street), the 83 (14th Avenue) and the 206 (12th Avenue Sutterville).

Despite the number of routes, there is no route that runs continuously along the entire Martin Luther King, Jr. Boulevard, and the portion between 12th Avenue and 24th Avenue has no service whatsoever. In addition, the bus stops offer little in the way of amenities to bus riders. Only three stops, including one in front of the community center, have a bench. Many other stops are poorly signed and many do not have adequate lighting to enhance rider safety at night. Access to bus stop locations is also impeded by obstructions along sidewalks.



Bus Stop at Martin Luther King, Jr. Boulevard & 12th Avenue





Overhead Utilities

Aerial utility lines are visible in much of the project area. Many of these lines run along alleys behind houses or along back fences in much of the South Oak Park Neighborhood. On Martin Luther King, Jr. Boulevard 22kV SMUD lines run along the west side of the street. Hanging lower on these same poles are generally 2-3 communication lines. Comcast and Surewest are known to be among these communication lines.

Discussion with community members has indicated a desire to relocate aerial utility lines along the corridor as much as possible. This particular corridor should be considered as a candidate for the SMUD shared-cost relocation program. With a request from the City Council, SMUD would help fund the relocation of aerial lines. In the future, the Martin Luther King, Jr. Boulevard corridor should be considered for this program as the 22 kV lines are of a low enough voltage that underground relocation is not cost prohibitive. Compared to similar projects, the cost for undergrounding the lines and making new service connections along Martin Luther King, Jr. Boulevard would be approximately \$2,250,000.

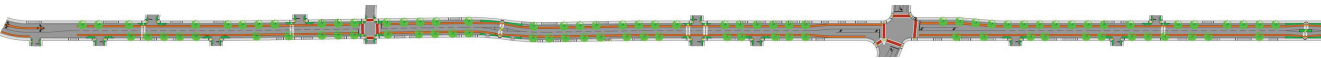
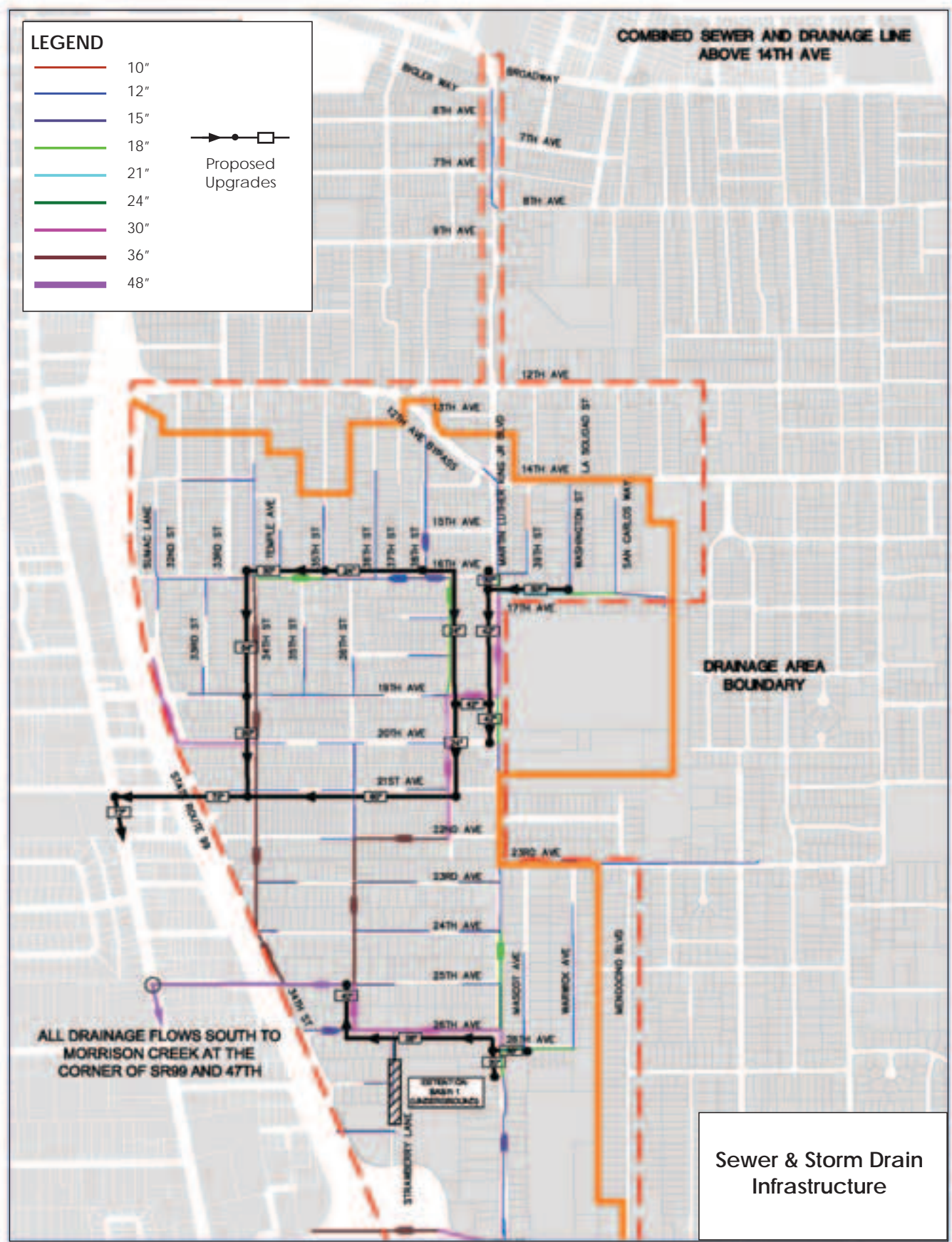
One of the sometimes forgotten costs for undergrounding utilities is reestablishing the connections from the home or business to the trench. Of the approximately 200 parcels that front the Boulevard half would need services from the trench. The other parcels are served from the poles at the rear of the properties.

It may be advisable to underground the communication lines as well. As construction of the improvements proposed in the Martin Luther King Jr. Boulevard Streetscape Master Plan proceeds, there will be an opportunity to construct a joint trench and conduit systems to house the communication lines that are currently aerial along the west side of Martin Luther King, Jr. Boulevard.



Overhead utilities create sidewalk obstructions and are unsightly





Underground Utilities

Martin Luther King, Jr. Boulevard has an extensive underground utility infrastructure that includes potable water, sanitary sewer, and storm drain lines.

Water is distributed through 8 inch pipes that run down 38th street as well as Martin Luther King, Jr. Boulevard. These 8 inch pipes connect to much larger 18 inch to 36 inch pipes that run down the north end of the boulevard and continue East down 14th avenue. The main pipes are located underneath the center of the existing roadways and should be accessible for any infill development proposed in the master plan.

The study area has both a combined sewer system as well as an independent sanitary sewer system. Most of the area is serviced by the independent system with a small portion to the north being serviced by the combined system. Studies done by West Yost and Associates in the November 2001 document "Basin 119 Sanitary Sewer Master Plan" have shown that the measured flow of the sewage down the 12 inch pipe spanning the length MLK is not severely restricted enough to warrant the immediate upsizing of the pipe.

The storm drainage infrastructure includes 3 different systems. These are Basin G252, Sacramento City's Combined Systems and Sacramento County's system. Generally, water drains from north to south in the project area. A majority of the project area drains into Basin G252. The main drainage pipe flows South down Martin Luther King, Jr. Boulevard then west across State Route 99 and South down Franklin Boulevard and eventually into Morrison Creek.

A Stormwater Master Plan for Drainage Basin 252 was completed by the City Utilities' department in March 2000. The Master Plan analyzed a majority of the study area south of 14th Avenue,



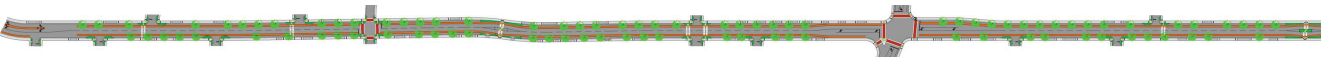
View of existing blocked storm drain inlet

but also included much of South Oak Park to the west of Highway 99. The Master Plan determined that there were deficiencies in the existing system giving it between a 2-year and 5-year storm frequency level or protection before nuisance flooding occurred with water reaching the top of the curb. Storm records for January 9th through January 12, 1995 storm events indicate that there were over 100 reported street flooding incidents with 4 that included property damage.

The Basin 252 Stormwater Master Plan identified a set of improvements to upgrade the system to minimize flooding for events up to a 10-year event and reduce the peak discharge into Morrison Creek. For the study area east of SR 99, the improvements include pipe size upgrades, minor redirection of flow and a detention basin at Strawberry Lane, see the Storm Drainage Figure. The total cost for these improvements escalated from the Masterplan's 1998 costs of \$7,000,000 at a rate of 3.5% to 2008 dollars is \$9,500,000. Only 15% of the improvements are in Martin Luther King, Jr. Boulevard which should be completed while the streetscape improvements are going on.

Additional information about the utility infrastructure is provided in the document Appendix.





3. Streetscape Plan

A Vision for Martin Luther King, Jr. Boulevard

Martin Luther King, Jr. Boulevard is already a well-used street. It accommodates heavy pedestrian, bicycle, and vehicular traffic. It contains mixed land uses and a diverse group of Sacramento residents. Because the existing condition of the streetscape is not very pedestrian or bike friendly, the fact that it works as well as it does is a testament to the resilience of residents of southern Oak Park. Sidewalks are narrow and commonly inaccessible; traffic speeds are unacceptably high; school drop-off areas create unsafe traffic situations; street trees are absent; and other street amenities such as bus shelters are non-existent.

The Oak Park community envisions a Martin Luther King, Jr. Boulevard that honors its name. A goal of this project is “to create the best Martin Luther King, Jr. Boulevard in the nation,” a place that residents are proud of and that creates a supportive environment for those that use it. The new Martin Luther King, Jr. Boulevard should be safe for all users. It should encourage walking and bicycling. It should also be beautiful and green.

The following plans show proposed improvements within the City-controlled right-of-way along the boulevard. The street is only a start, however, and the success of any streetscape project also depends on the land uses, open spaces, and general economic conditions of the entire neighborhood.



Proposed streetscape improves the public right-of-way, creating a pleasing environment for all of its users

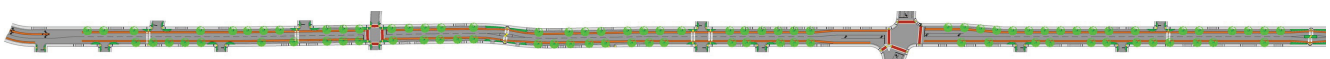


Design Standards

This section outlines important information regarding many of the street design decisions for the Martin Luther King, Jr. Boulevard Master Plan. There are several proposed design details that differ from typical design standards for the City of Sacramento. The proposed design of Martin Luther King, Jr. Boulevard is generally consistent with the objectives for city streets outlined in the beginning of the Sacramento Street Design Standards. Because of the unique physical and cultural context of Martin Luther King, Jr. Boulevard, some minor deviations from city standards are warranted. The following excerpt from the Street Design Standards is included for reference:

"The City's street system should encourage alternate mode use, especially walking and bicycling, by working toward a balance of all street users. To achieve this, the City has identified the following objectives for the city's streets:

- Residential streets and street networks should be designed to discourage speeds above 25 mph.
- Residential street networks should be designed to encourage only neighborhood traffic and should seek to minimize traffic volumes on residential streets.
- Street design should enhance and improve the pedestrian safety and comfort and encourage non-motorized travel modes.
- Employ traffic calming measures when the size and/or shape of a residential subdivision project limits the number of alternative designs.
- Discourage parking on sidewalks.
- Enhance and beautify the streetscape and pedestrian environment by bringing landscaping closer to the street.
- Balance street design so that it does not favor motorized traffic.
- Streets should not be barriers to personal interaction."





Existing photo along Martin Luther King, Jr. Boulevard



Rendering of proposed improvements



Street Cross Sections

The limited right-of-way along Martin Luther King, Jr. Boulevard presents a significant design challenge. Presently, the land available to accommodate sidewalks, tree planters, parking, bicycle lanes, and automobile lanes is limited to 58 feet in some locations. The large number of small parcels along the length of the boulevard makes acquisition of additional right-of-way infeasible. Because of this, ideal circumstances may not exist for all elements of the street cross section. Priority has been given to pedestrians, bicyclists, transit riders, and finally to automobiles. This approach is consistent with the expressed desire of residents and most project stakeholders.

The cross section shown on the facing page is the result of numerous meetings, workshops, and internal discussions and represents a realistic compromise that reasonably accommodates all of the project goals.

Sidewalks are an important element of a healthy street. Martin Luther King, Jr. Boulevard already has a high level of pedestrian activity which is supported by numerous schools, businesses, and residents who choose to walk rather than drive. Current sidewalks are 5 feet wide and are obstructed by power poles and other obstructions. A 7-foot wide sidewalk is recommended, along with the undergrounding of utility lines to make the walking environment more pleasant and handicap accessible. As part of this project, existing non-ADA-compliant driveways would be rebuilt to current standards and would not be borne by area land owners.

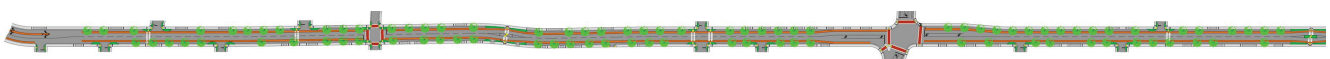
The corridor does not currently have street trees. This creates an unpleasant environment for pedestrians and causes increased "heat island" effects which raise ambient temperatures of roads, walkways, and parked vehicles. Street trees are recommended and are proposed to

be located in tree wells that appear within the parking area of the street. These tree wells can only be the width of a parking stall and therefore will only best accommodate small to medium canopy trees.

Parking areas are retained in the street right-of-way and are recommended to be 7 feet wide. The number of parking spaces is reduced in the plan by the placement of trees; however, many of the existing spaces are currently not used.

The current street has double-striped five-foot wide Class II bicycle lanes. This plan recommends the same configuration, but with integrally-colored asphalt. This will make the bicycle lanes more visible; it will also create the perception of a narrower vehicle lane which serves to slow automobile traffic. Colored bicycle lanes have been used successfully in many cities but come with an added installation cost and additional maintenance concerns. The significance of this boulevard justifies some additional cost, but further cost studies should be pursued prior to installation.

Vehicle lanes are currently 12 to 14 feet wide. Wide travel lanes encourage speeding and add unnecessary paving. Because this corridor is served by bus lines and is also home to a busy fire station, the lanes must still accommodate some larger vehicles. This plan recommends 11 foot travel lanes along the entire length of the boulevard. 11-foot lanes are common in situations where existing rights-of-way are limited and can be found throughout Sacramento on streets such as Folsom Boulevard, which also accommodates bus and other heavy traffic.

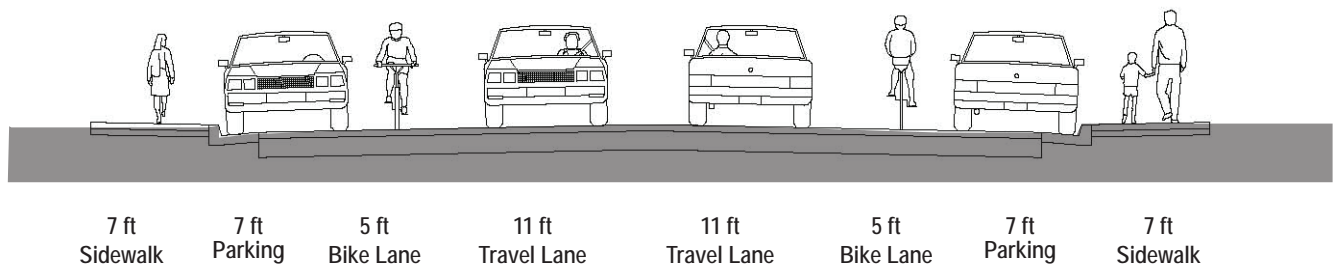


Cross Section on Martin Luther King, Jr. Boulevard at Tree Wells

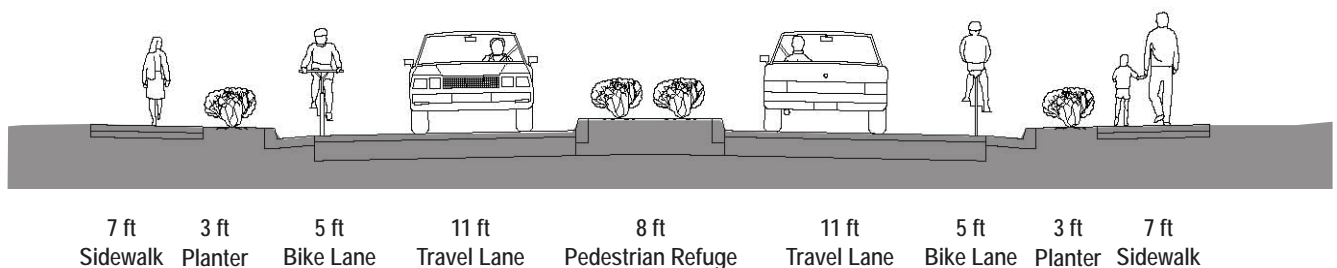


The proposed cross section on Martin Luther King, Jr. Boulevard includes reducing the curb to curb width to 32 feet where there are planter bulb-outs and 46 feet typical throughout the remainder of the corridor.

Cross Section at Parallel Parking Areas



Cross Section at Pedestrian Refuge Areas



Travel Lanes

Since the 1950's, roadways in the United States have been designed with the primary objective of moving cars quickly from one point to another. Often, little consideration has been given to traffic calming or to providing safe alternative routes for bicyclists and pedestrians. The existing travel lanes on Martin Luther King, Jr. Boulevard are between 12 and 14 feet wide. Anecdotal evidence suggests that traffic tends to speed up along portions of the boulevard that are widest, such as in front of Father Keith B. Kenney Elementary School.

In an effort to provide additional sidewalk width, this plan recommends the reduction of lane width to 11 feet. Eleven feet is an acceptable width for the operation of buses and emergency vehicles. Additionally, narrower lanes have been shown in numerous studies to reduce the speed of travel by affecting the psychology of drivers.

Bike Lanes and Parking Lanes

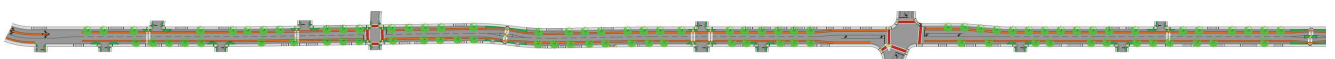
The Martin Luther King, Jr. Boulevard cross section includes 5-foot bike lanes adjacent to 7-foot parking lanes for a combined 12 feet, one foot narrower than the recommended combined width of 13 feet in the City's standards. Nationally, 12 feet is generally recognized as the narrowest appropriate width of a combined bike lane and parking area. This width is commonly used on street restriping or reconstruction projects where right-of-way width is constrained. The proposed design incorporates colored asphalt in addition to the standard bike lane stripe to highlight the bicycle facility. There are two major benefits to the colored treatment. First it helps calm the traffic lanes by making them feel narrower and adding side friction. Secondly, the colored pavement will clearly demarcate the parking zone, helping to keep parked cars from encroaching to the bike lane.



Narrower lanes can be an effective traffic calming tool



Double-striped, colored bicycle lane and on-street parking



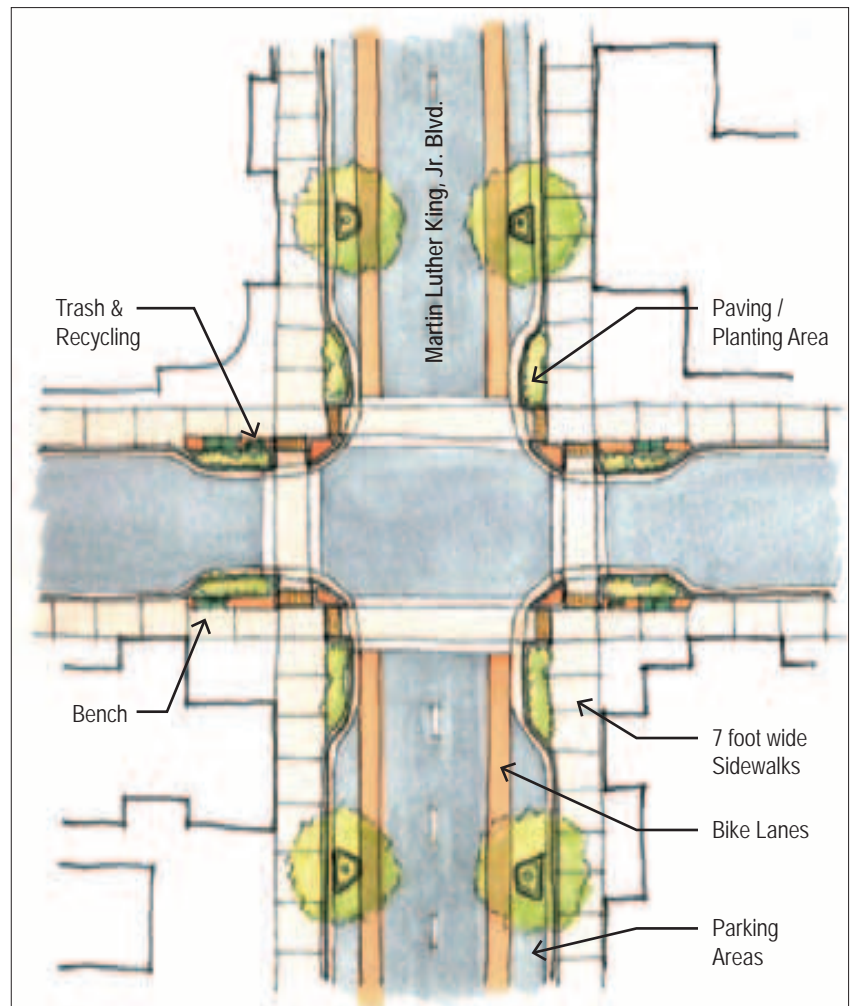
Sidewalks and Bulb-outs

The sidewalk has been increased from the existing 5 feet to 7 feet all along both sides of Martin Luther King, Jr. Boulevard. A seven foot wide sidewalk will create a much more accepting and desirable pedestrian experience. The planter bulb-out area between the sidewalk and the curb is an important element, which helps achieve several of the City's objectives for city streets. In particular, this feature enhances and improves pedestrian safety and comfort, and encourages personal interaction. The bulb outs will result in a crossing distance of 34', reduced from the current 46', and provide improved pedestrian visibility for oncoming motorists. Currently, crossing pedestrians must enter the traveled way from areas that are, at times, blocked from view by on-street parking.

At key intersections, bulb-out areas can be treated in a variety of ways to accommodate benches, trash and recycling receptacles, public art, bus shelters, and landscaping. In lieu of plantings, pavers or cobblestones can be installed, which may be removed easily at a later date if adjacent land-owners or businesses are willing to assume maintenance of the plantings.



Bulb-outs improve safety for pedestrians



Typical Intersection with Crosswalks and Bulb-outs



21st Avenue Improvements

21st Street is a critical secondary road that runs east to west, from Martin Luther King, Jr. Boulevard at Oak Ridge Elementary to Franklin Boulevard and beyond to the west. It is the only connection across the freeway between 12th Avenue and Fruitridge Road. As such it acts as a gateway into Oak Park from the west. It also accommodates a significant amount of “cut-through” traffic and also a bus route.

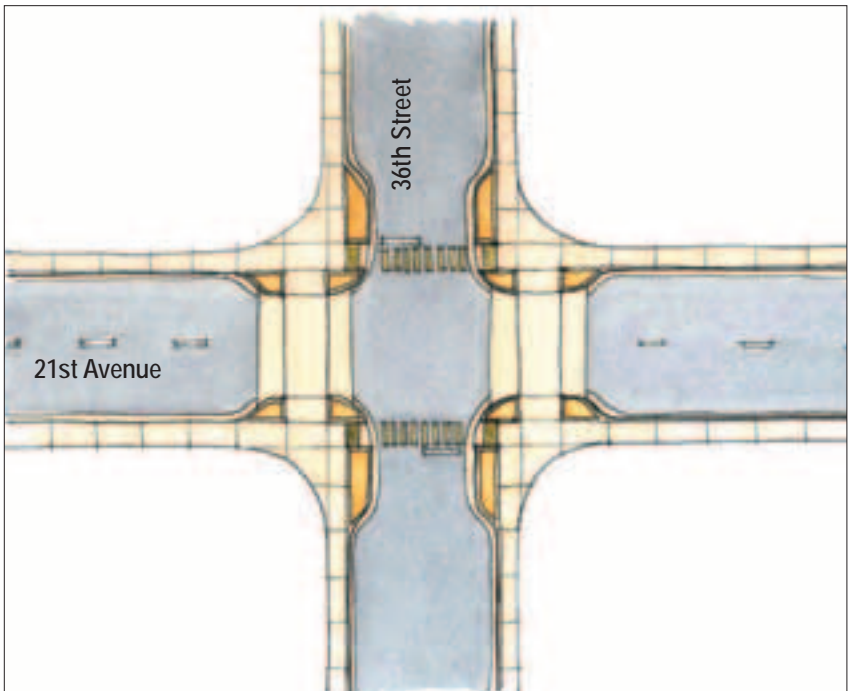
Within the roadway’s 45-foot right-of-way are 4 1/2-foot sidewalks, 3-foot rolled curbs, and 15-foot combined travel and parking lanes. Pedestrian-scaled street lighting is intermittently but not located at all critical areas such as bus stops. There are no street trees except those planted by residents within their yards.

The 45-foot right-of-way does not allow for significant changes to be made to the road. However, the following recommendations would improve safety and beautify the neighborhood.

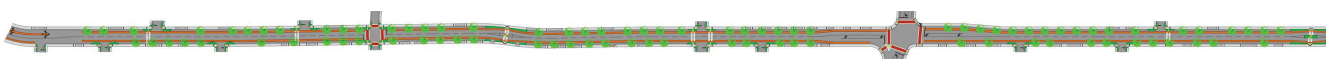
The improved roadway would include 5 1/2-foot sidewalks, vertical curbs, and 17-foot combined travel and parking lanes. Pedestrian-scaled lighting should be added to augment the existing lighting. The three intersections would be modified with bulb-outs and crosswalks. These serve as a traffic calming device, to focus the driver's attention to the pedestrian crosswalks. Crosswalks can be made safer by using specialty paving, more visible painting, or by adding speed tables.



View of typical intersection along 21st Avenue



Typical Intersection along 21st Street with Bulb-outs and Crosswalks



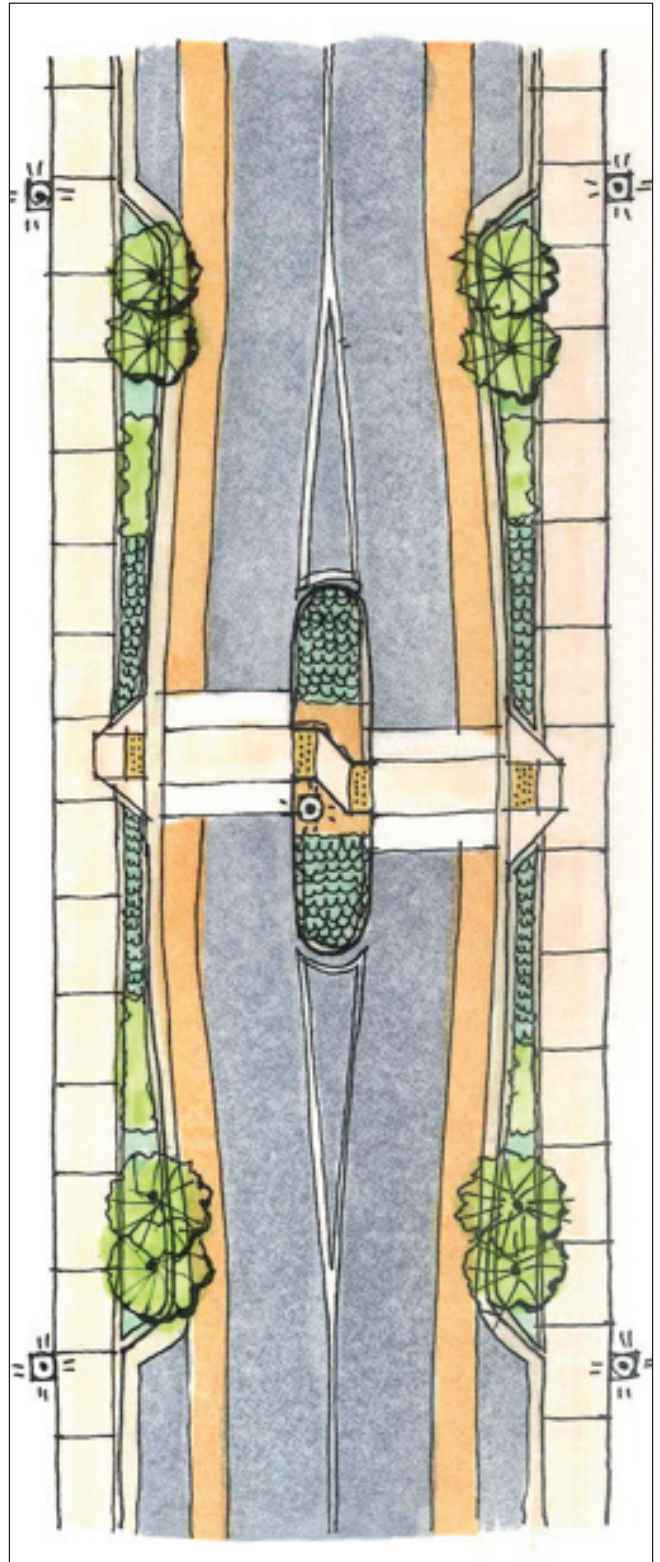
Pedestrian Oasis

A pedestrian oasis is recommended at Christian Brothers High School, the currently undeveloped "opportunity location" at the south entrance to the corridor, and at Father Keith B. Kinney Elementary School. A pedestrian oasis provides a shelter where pedestrians may wait outside the traveled way until vehicular traffic clears, allowing them to cross the street in two stages. As a split pedestrian oasis, with staggered crossing locations requiring crossers to look toward oncoming traffic, they provide pedestrians a better view of oncoming traffic and to allow drivers to clearly see pedestrians. They are particularly useful to persons with mobility disabilities, very old or very young pedestrians who walk at slower speeds, and persons who are in wheelchairs. Wheelchair users need adequate width and level areas for waiting on the refuge.

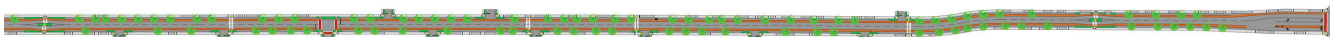
The aesthetic treatment of the pedestrian oasis can vary depending on the cost and the ability to fund the maintenance of landscaping.



Photo of pedestrian oasis with staggered pedestrian approach.



Conceptual sketch of proposed Pedestrian Oasis



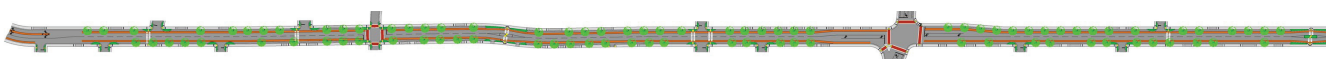
Street Trees

The street trees for Martin Luther King, Jr. Boulevard should be medium-sized deciduous or broad-leafed evergreen shade trees. The species selected should be taken from the City of Sacramento approved street tree list. The same street tree species should be used for contiguous sections of the street, and when a species change occurs, it should be at major intersections or other defining breaks in the street pattern. The tree wells, when they occur in the parking lane of the street, are to be curbed, and of an adequate overall dimension to allow parking cars to strike the curb before striking the trunk of the tree. The trees shall be planted in the center of the tree well opening, and installed per the City of Sacramento approved tree installation details.

Street tree maintenance will initially require annual fertilization and pruning. Once the trees are established and the pruning has opened and shaped the trees into their natural character, less fertilizer and maintenance will be required. Depending on the species used, the irrigation system may be turned off after several years of plant establishment. An annual inspection for disease, and remediation, if required, should be conducted. After a two year establishment period, the tree stakes and ties should be removed to avoid girdling of the tree trunks.



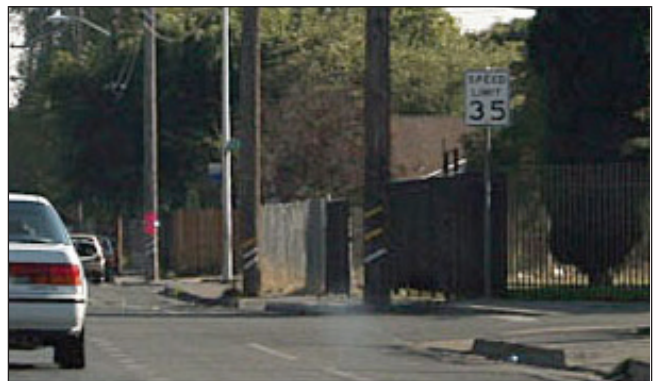
Typical Tree Wells



Design Speed

The proposed street design has taken in account the desire to lower vehicular speeds on Martin Luther King, Jr. Boulevard. Elements have been added to the design to help calm traffic. Key traffic calming elements include reduced lane widths, bulb outs, tree planters, and colored bike lane. Martin Luther King, Jr. Boulevard most closely fits the street designation of a Major Collector Street as outlined in the City Street Design Standards.

The intent of slowing traffic speed is to achieve an appropriate balance between travel modes for this important collector street with mixed residential, commercial and institutional uses. This is consistent with objectives a, c, g, and h from the City's Street Design Standards.



Speed limits vary from 35 mph to 25 mph at school zones

Alleys

As indicated earlier, many portions of the project area, especially in the south (south of 16th Avenue), alleys are very long and do not serve many existing buildings. Because of security and maintenance issues this plan recommends the abandonment of these alleys. City right-of-way would be given to property owners, but public utility easements would be put in place to allow for maintenance and service. Fences would be relocated to the center of existing alleys. Before any alley is abandoned the city should meet with adjacent property owners and tenants and evaluate pros and cons of the closure. For a full discussion on the City's process go to: <http://www.cityofsacramento.org/dsd/development-engineering/vacation-abandonment/>



Street Furnishings

This master plan aims to improve the function and the beauty of the boulevard. Emphasis is placed on creating an inviting space at the pedestrian scale.

- Provide Acorn-style lighting at a pedestrian scale throughout the boulevard.
- Space pedestrian lighting at approximately every 150 feet.
- Install permanent artistic banners that commemorate the community and /or the life of Dr. Martin Luther King, Jr.
- At major stops, including the community center, provide uniquely designed, covered bus shelters that include benches, route maps, schedules, lighting, trash, and recycling receptacles.
- At all bus stops, provide a bench, route maps, schedules, and pedestrian-scaled lighting.
- Coordinate street furnishings with Acorn-style fixtures. Consider using a color such as green or brown rather than black, to provide a distinct character.



Bus Shelters (also provide maps, schedules, trash & recycling)



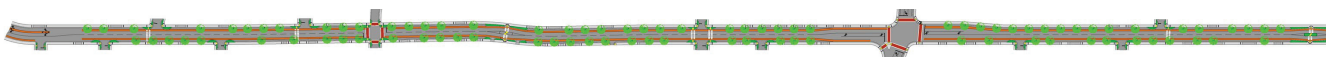
Benches



Acorn-style pedestrian lighting



Banners



Paving Materials

Material textures and colors can enhance the character of the environment and need not be cost-prohibitive. Simple scored concrete walkways are adequate through most of the corridor. At special nodes, such as bulb-outs, pedestrian crossings, public parks and plazas, specialty paving should be used. Brick, concrete pavers, and stone are appropriate materials that provide a resilient and accessible surface.

Pedestrian Crosswalks should be marked to be immediately visible to drivers and cyclists. Throughout the corridor, City-standard "triple-four" markings are appropriate. These are simple painted crosswalks with wide stripes 4' deep in dimension. At special nodes, such as school pedestrian oases, the community center, 12th Avenue, and 14th Avenue, specialty paving should be considered.



Colored concrete pavers at specialty locations



Scored concrete sidewalks



Brick pavers at specialty locations



Integral colored bicycle lanes



Stamped crosswalk paving

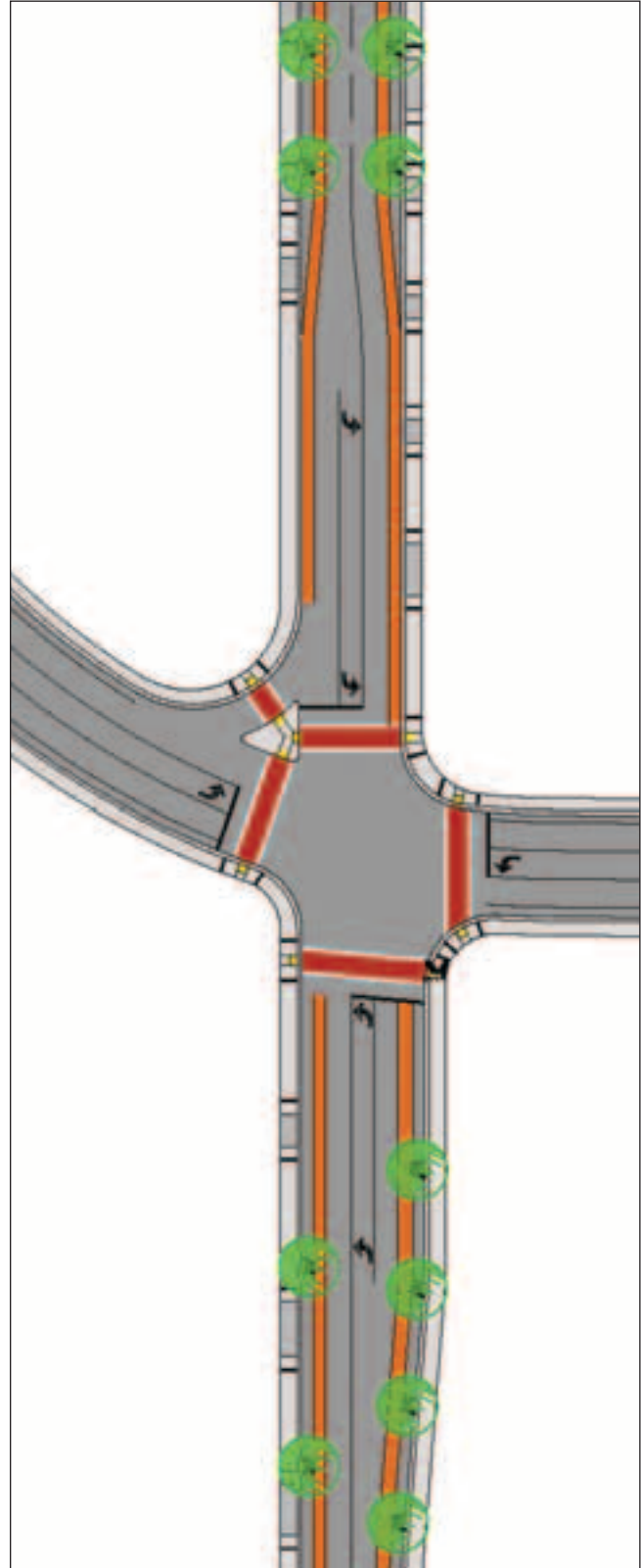


Realignment at 12th/14th Avenues

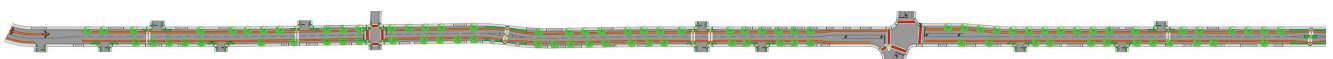
The proposed design for this intersection is a more compact pedestrian friendly signalized intersection with one fewer lane in each direction than exists today on 12th Avenue / 14th Avenue Bypass. This street is only 2 lanes on 12th Avenue west of 36th Street and on 14th Avenue east of 39th Street. It is unlikely that 12th or 14th Avenues will be widened in the near future, which justify maintaining 4 travel lanes in this area. A roundabout was studied for this intersection, but due to high turning volumes a double lane roundabout would have been required and the right of way impact for a such a facility makes it impractical.

In order to accurately assess the traffic conditions resulting from the lane reduction at this intersection, traffic counts were collected. Turning movement counts were taken in the morning from 7 AM to 9 AM and in the afternoon from 3 PM to 6 PM. These times attempted to catch the peaks created by the nearby schools in addition to the peaks created by commute traffic.

Detailed traffic counts, including turning movements, were taken in the fall of 2007 to analyze the feasibility of both the roundabout and the signalized intersection. The current intersection configuration operates at a Level of Service (LOS) C. The proposed pedestrian friendly signalized intersection will operate with a LOS D with current traffic volumes, which under the current general plan update is acceptable as a minimum level of service. A pending update of the General Plan proposes to allow LOS D levels, however. Although there has not been any traffic forecasting done for this intersection, it is not anticipated that the volumes will increase in the future to cause the intersection to fail. Trends over the last 15 years have not shown any significant increase in traffic volumes.



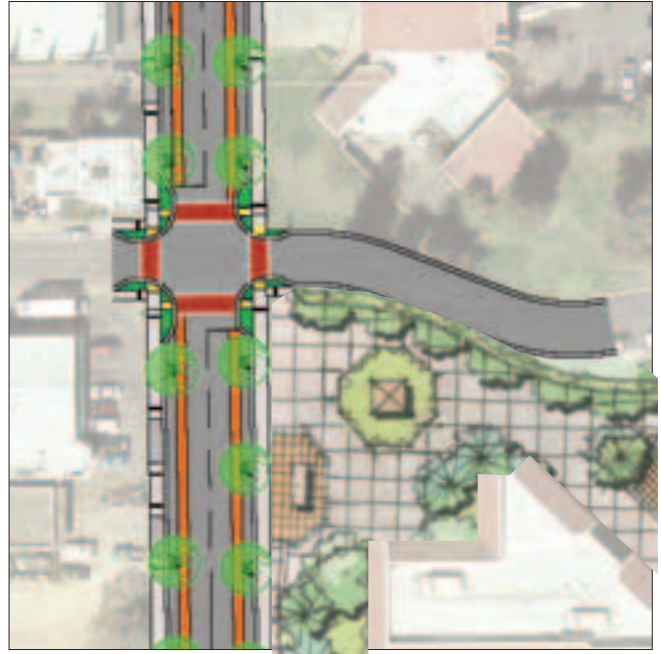
Proposed intersection realignment at Martin Luther King, Jr. Boulevard and 12th /14th Avenues



Realignment at 9th Avenue

A signalized Tee intersection currently exists at Martin Luther King, Jr. Boulevard and 9th Avenue. Just to the north of this intersection is the fire station, and just to the south is the existing uncontrolled driveway into the Oak Park Community Center. Because of these existing conditions, the safety and aesthetics of this intersection leave much to be desired. This is also one of the busiest, most active areas along the corridor, and it is the heart of community life with the community center at one of the corners.

This plan proposes to realign the Oak Park Community Center driveway with 9th Avenue. This will provide a safer and more appropriate intersection and entryway into the community center. The change will better manage traffic coming in and out of the community center. This will create a safer environment along the street and especially for the frequent emergency response calls from the adjacent fire station. It also opens up a significant amount of land in front of the community center, which would be usable as a plaza area for activities and to create a more beautiful view into the facility.



A safer and more attractive entry to the Oak Park Community Center at 9th Avenue

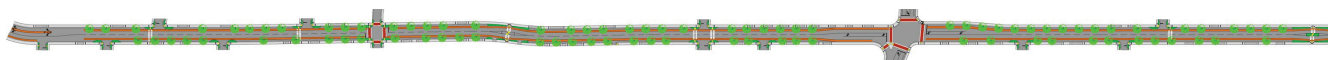
Transit

The City should work with Regional Transit to secure a transit route that services the entire length of Martin Luther King, Jr. Boulevard and transit stops that are befitting the redesign of the corridor.



Provide transit service the entire length of the corridor





4. Landscape Concept Plans

This chapter explores several possible landscape improvements to the study area. These include parks, gateways, and a community garden. As discussed in the Existing Conditions chapter, the study area is currently deficient in its provision of park acreage to area residents. These concept plans provide ways not only to improve the quality of existing facilities but also to provide new facilities that will enrich the lives of residents.

Conceptual Designs for the following sites follow:

- Oak Park Community Center
- Memorial Park
- West Gateway
- Temple Park
- 24th Avenue Park
- Community Garden

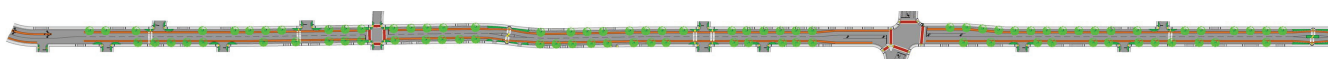


Community Center

The design concept for the Community Center is aimed to accommodate the natural flow of pedestrians to the Center from the parking lot and Martin Luther King, Jr. Boulevard, creating a multi-purpose plaza, enriching the character of the landscape and microclimate and creating an artistic or architectural monument adjacent to the street. The concept articulates special use areas, such as the multi-purpose plaza and bus shelter areas with concrete pavers. The remainder of the paved area is integrally colored and scored concrete pavement. Benches are placed adjacent to the Community Center entry, and shade trees are located in the plaza area to modify the micro-climate and place a "roof" over a section of the plaza to allow for hot weather comfort. Shade trees line the entry drive, and ornamental trees help to define the points of entry to the buildings. The planting beds contain a unifying evergreen groundcover.

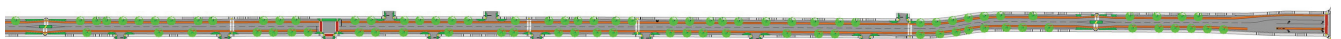


Existing aerial view





Conceptual Site Plan



Memorial Park

The design concept for the Memorial site at the intersection of 12th Avenue and Martin Luther King, Jr. Boulevard addresses three of the four corners of the intersection. The largest area is in the northwest corner and can provide a range of artistic opportunities that will memorialize and celebrate the life of Dr. King. Artistic opportunities are diverse and can be integrated into the design in a variety of ways.

The concept articulates special use areas with concrete pavers, such as the pedestrian point of arrival and access into the site from the intersection. Integrally colored and scored concrete paving articulates the pedestrian desire lines through the site. Ornamental trees define the point of arrival from the intersection, and shade trees define the street edges. Evergreen trees, in combination with a decorative wall or fence, define the northern edge of the site. Turf provides a large, passive multi-purpose space for community events and the planting beds contain a unifying evergreen groundcover.

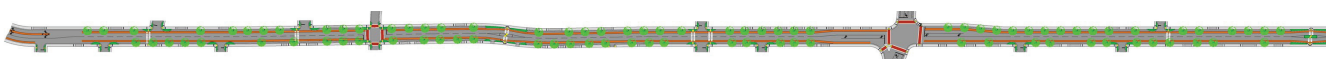
In the southwest corner of the intersection is a small, passive neighborhood park site oriented to the adjacent neighborhood. This space provides a shaded seating plaza with a seat-wall that separates the plaza from 12th Avenue. The plaza and intersection areas are defined with concrete pavers, and the sidewalks are integrally colored and scored concrete. A concrete walkway from the neighborhood provides easy access to the plaza and intersection. Shade trees help to define the plaza space, evergreen trees are used to separate the park site from the adjacent neighbors to the west, and ornamental trees help to define the pedestrian point of arrival at the intersection. The planting beds contain a unifying evergreen groundcover.

In the southeast corner of the intersection is an existing public use area. The design concept



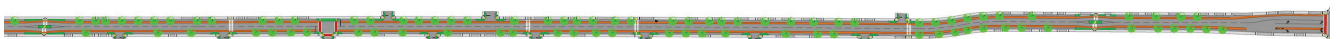
Existing aerial view

proposes modest landscape, paving and seating improvements, while protecting the integrity of the existing design. This is primarily a pass-through space that includes benches, evergreen trees to separate the space from adjacent neighbors to the south and east and a unifying evergreen groundcover.





Conceptual Site Plan

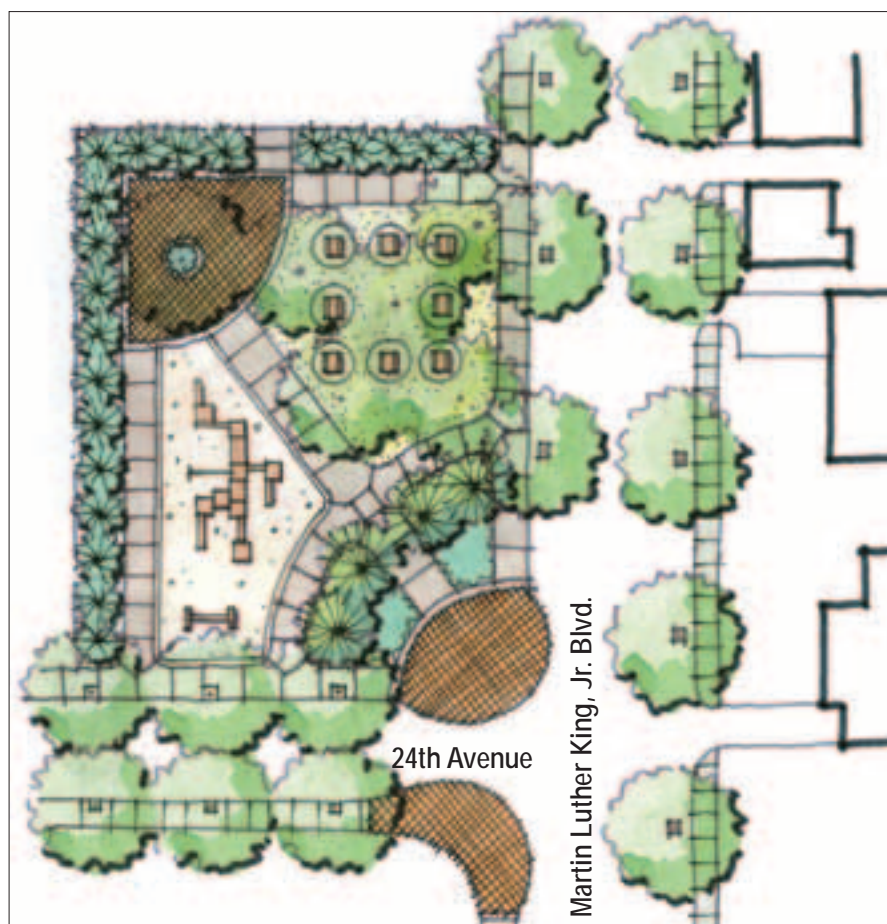


24th Avenue Park

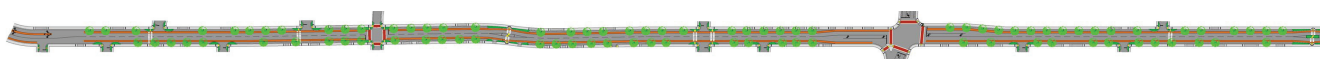
This neighborhood park is aimed at addressing some of the park and recreational needs of the adjacent neighborhood, while providing a picnic grove and recreational activities that could be used by the adjacent church to the north. The site is approximately 0.4 acres; the design concept includes a children's play area, a shaded picnic grove, and a seating plaza. Concrete pavers are used to announce the point of arrival at the intersection. The seating plaza has a concrete seat-wall situated on concrete pads in a field of turf. The point of arrival at the intersection is defined by ornamental trees, and the eastern and northern edges of the site use a columnar evergreen to screen the site from the adjacent neighbors. The planting beds contain a unifying evergreen ground cover. The feasibility of land acquisition still needs to be explored.



Existing aerial view



Conceptual Site Plan



Temple Park

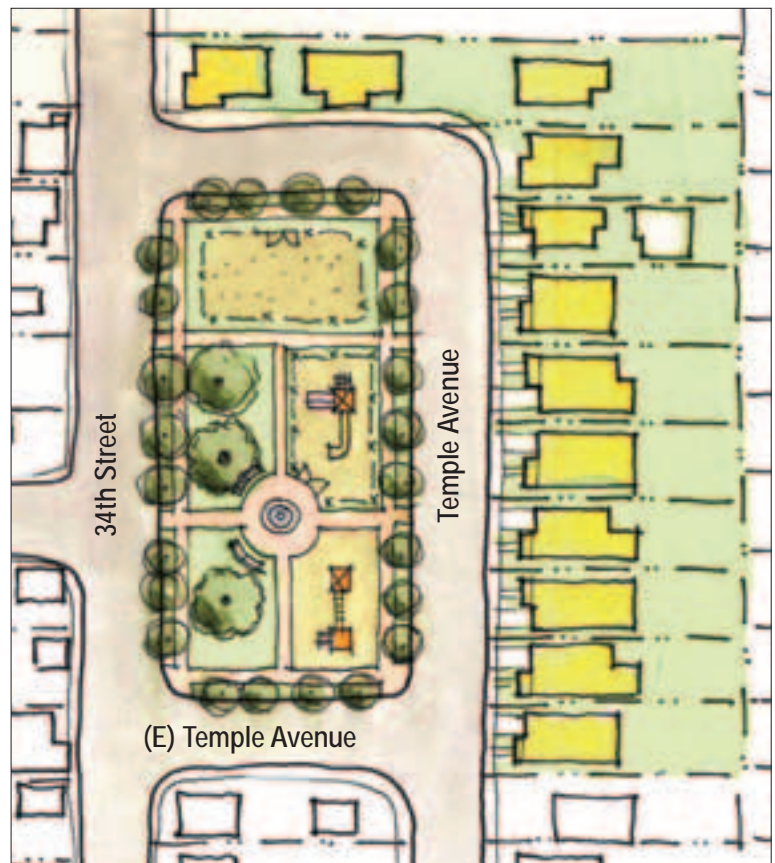
This existing one-acre park provides the only public recreation area in South Oak Park. The existing park has green open space and a tot play structure. It is a needed amenity for the area. However, it is also poorly sited. Half of the park is surrounded by back yard fences on three sides. Surveillance is poor, especially at night. The park is bounded by residential parcels that are extremely deep, up to 300 feet in length.

The sketch plan below illustrates one approach to improving this amenity for the community. This concept would require a re-configuration of the park as well as a few adjacent parcels. The benefits of this approach are to create a park that is open on all sides and that would be fronted by new single family homes.

Further study of this park and its enhancement is recommended.



Existing aerial view



Conceptual Site Plan



Community Garden

A community garden can be a valuable part of a public park system. It can be an opportunity for residents to grow their own healthy food; it can create a destination for walks through the neighborhood; it can nurture a sense of community by creating a common goal; and it can be used as an educational tool in conjunction with local schools.

A garden may be feasible in many locations but will be most successful if it is located along a public street, with good surveillance. Locating a garden next to a school can also create additional opportunities for education.

Possible locations include:

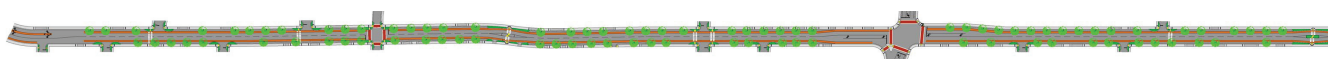
- Corner of 12th Avenue and Martin Luther King, Jr. Boulevard
- Lot adjacent to Temple Park
- 3440/3448 Martin Luther King, Jr. Boulevard



Community Gardens create healthy food opportunities



Conceptual Site Plan

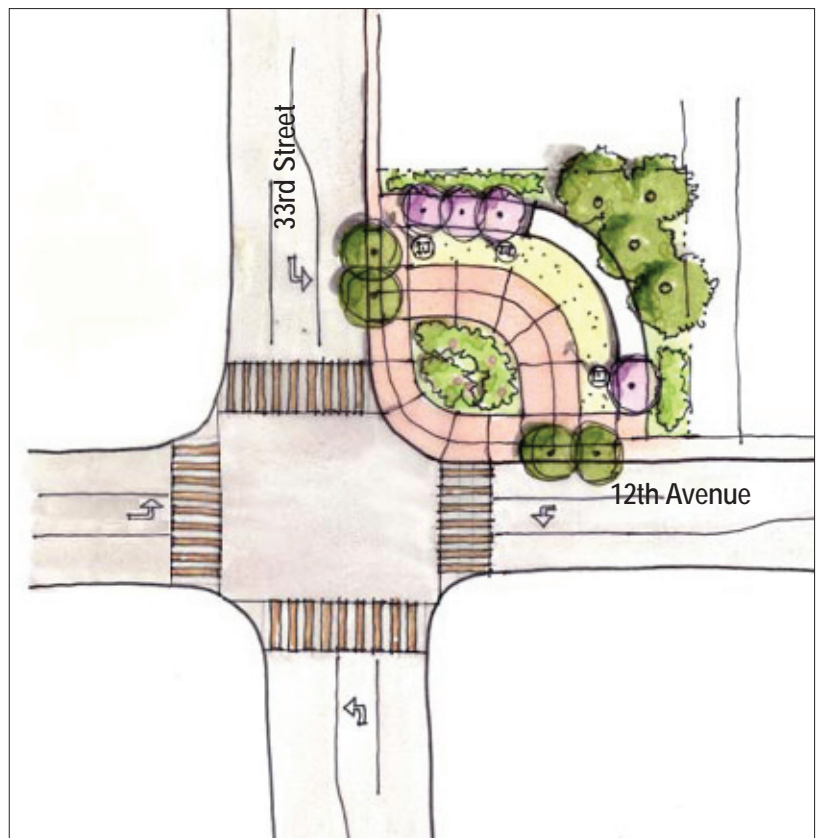


West Gateway

The intersection of 12th Avenue and 33rd Street is a gateway from Highway 99 into Oak Park. Currently, it is flanked by a gas station and a vacant lot. This location is ideal for a gateway landmark at the northeast corner of the intersection. The vacant lot could be developed as a passive landscaped area, or it may be developed with the intent of providing a venue for various uses.



Existing aerial view



Conceptual Site Plan



21st Avenue Gateway

The underpass at 21st Avenue and State Route 99 provides a unique opportunity. The underpass is currently an uninspiring entry point into the community. Simple treatments to the concrete structure could celebrate the sense of arrival into Oak Park and also be resistant to vandalism.

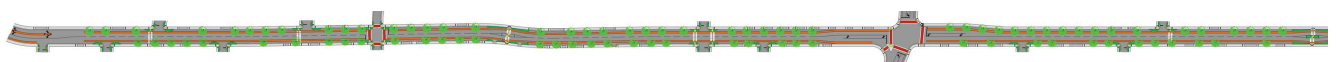
Safety improvements are also proposed, including narrowing travel lanes, striping bicycle lanes, removing parking, widening sidewalks, and providing better lighting.



Existing tunnel looking east



The 21st Avenue tunnel provides a gateway opportunity for public art.



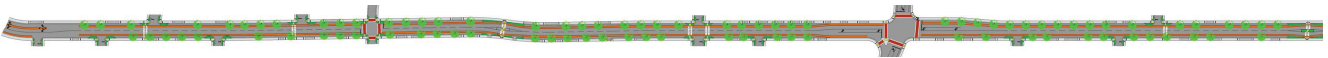
5. Development Opportunity Sites

Significant development opportunities were identified and studied for their potential to enhance the community socially and economically. Conceptual site plan sketches were prepared for several vacant sites, as shown on the plan on the following page. Four specific sites were studied in greater depth to explore development possibilities that could support community revitalization. There are no concrete plans to develop these sites; however, these should guide staff and the community in future planning and analysis of proposals.

Four Development Concept Plans follow:

- 9th Avenue Infill – a mixed-use development composed of six attached single family homes, ten townhouse/loft units and a flexible commercial space could accommodating a grocery store or two to three other retail uses.
- Coke Site – a potential residential development of ten townhouse/loft units.
- Four Corners Site – a potential residential project with 21 townhouse/loft units.
- South East Martin Luther King, Jr. Boulevard Neighborhood Site – a potential 57 unit single family subdivision with shared common open space.





9th Avenue Infill

This concept plan shows development on 8 existing parcels that total about 1 1/2 acres. Approximately one acre is shown as residential development, and one half acre is commercial development. The zoning is a mixture of Residential and Limited Commercial.

This site is directly opposite the Oak Park Community Center, at the civic heart of this community. As such, it sets the mood for the community. Currently, it includes a convenience store and a lawn mower shop, plus several vacant properties.

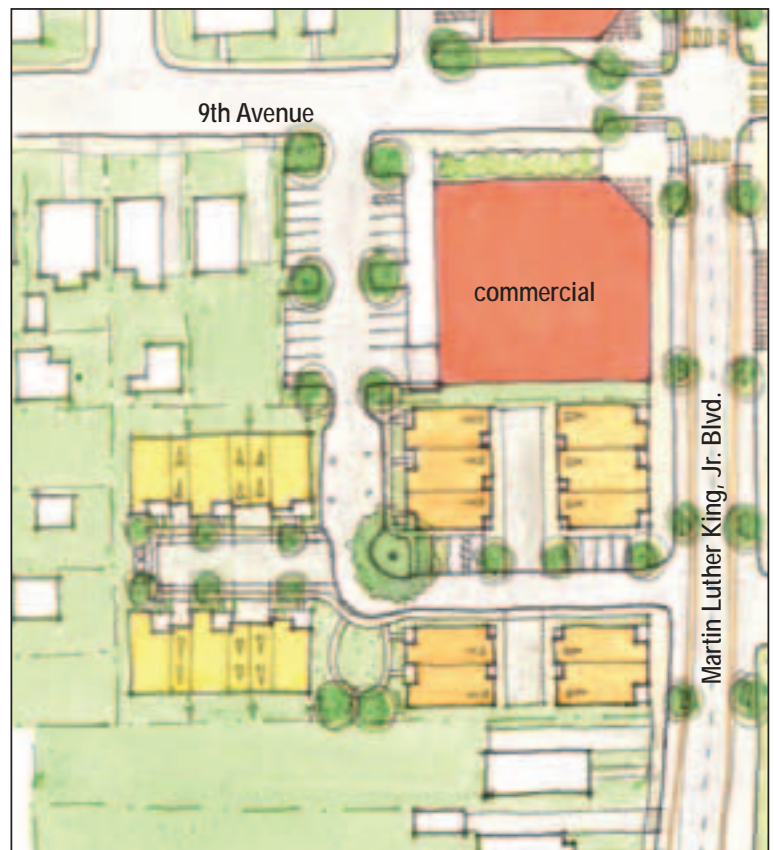
The concept sketch envisions the redevelopment of the existing commercial parcels at the prominent intersection. It recommends the relocation of the parking lot from the street frontage to behind the building.

On the four deep, vacant parcels to the south, infill housing is shown. These housing types are two story attached townhomes. They create a pleasing face to Martin Luther King, Jr. Boulevard with front porches and windows that overlook the sidewalk. Garages are located in the rear. Behind the townhomes are slightly larger homes with small private yards. There are 16 housing units in total.

Access for the residential units and for the retail portion of the project is from one driveway on Martin Luther King, Jr. Boulevard and one driveway on 9th Avenue.



Existing aerial view



Conceptual Site Plan



Coke Site

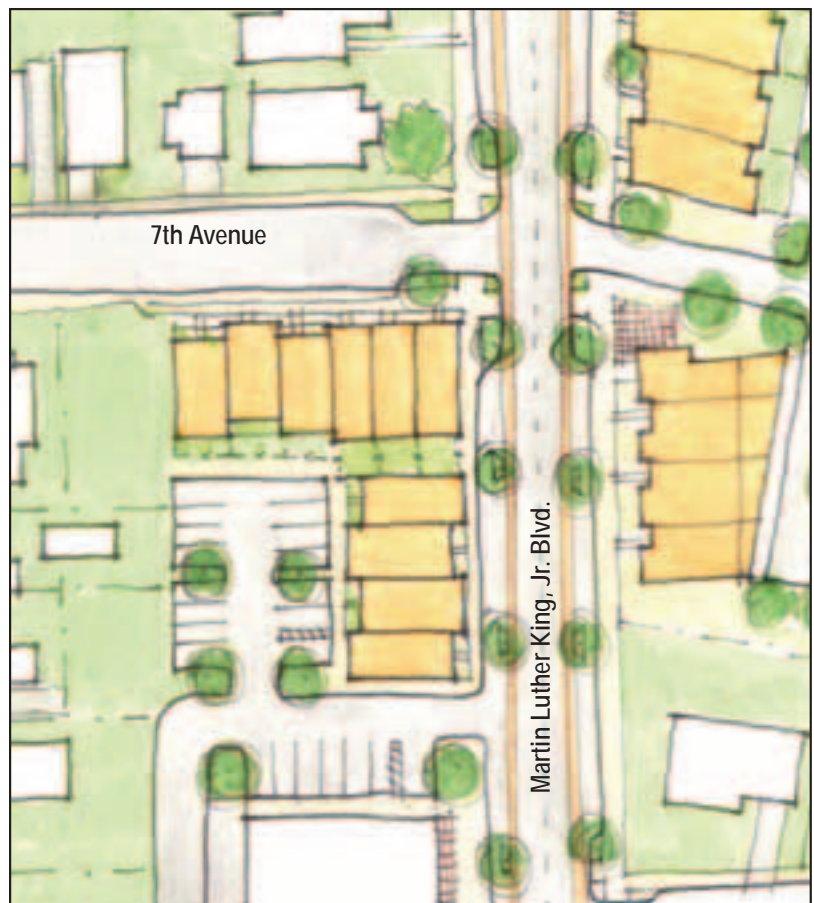
This site contains three parcels and totals .43 acres. One older commercial building occupies the southernmost parcel and the other parcels are vacant. All of the parcels are zoned for residential duplex (R-2) uses.

This site is just north of the Market site and is also close to the community center. It is also close to Broadway and to mixed use areas. Currently, there is a one-story structure on one of the parcels and no buildings on the remaining parcels.

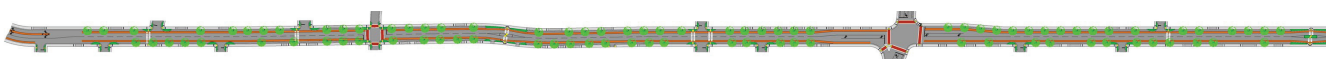
The proposed site plan shows two-story townhomes fronting Martin Luther King, Jr. Boulevard as well as 7th Avenue. These townhomes have small private patios. They have front doors that face the public streets, and rear entries that face a common parking area. There are 10 housing units in total.



Existing aerial view



Conceptual Site Plan



Four Corners

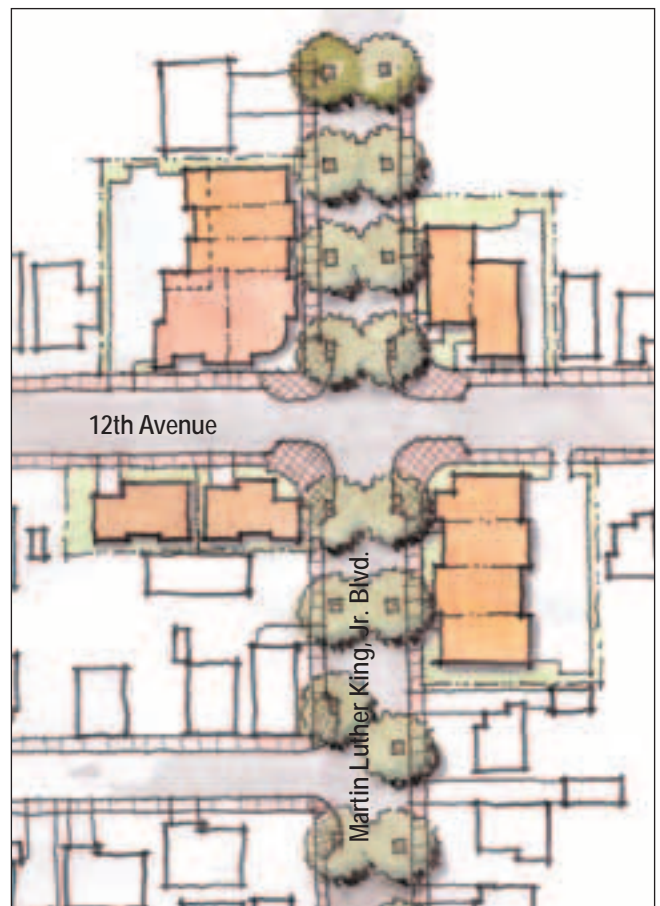
This area includes four vacant parcels on each of the four corners of the intersection of Martin Luther King, Jr. Boulevard and 12th Avenue. These parcels combine to total .85 acre of land. All four parcels are currently zoned for limited commercial uses.

The two larger parcels once contained gas stations. The commercial land uses were more appropriate before the 12th / 14th Avenue bypass was built. Today, retail commercial uses are not as viable. The northwest parcel is currently owned by SHRA and has already undergone site cleanup to remove hazardous waste from the former gas station. The southeast parcel will likely still require substantial cleanup.

The proposal calls for 19 housing units on the four corners. This would be best undertaken as one development project in order to achieve the best economy of scale. These units are envisioned as potential “live-work” units, meaning they could have residential or home-office uses at the ground floor. The buildings would face the street with parking tucked behind if possible.



Existing aerial view



Conceptual Site Plan



Southeast Neighborhood

This area is currently vacant land on four contiguous parcels, totaling 7.3 acres. The largest parcel contains public storage uses on its southern half. The site is otherwise bounded by single family residences and one apartment building on Martin Luther King, Jr. Boulevard. Vehicular access to the site is available along 26th Avenue. One additional access point would need to be provided along Martin Luther King, Jr. Boulevard and is identified on the concept sketch in the location of the one vacant residential parcel.

The concept plan includes a total of 57 houses. These houses are a combination of small detached and zero-lot line houses. A street network connects the new neighborhood to the existing community and provides a safe and navigable roadway for residents and emergency vehicles. Roadways also anticipate potential development from the neighborhood to the south. Housing units all face the street with porches and windows. Driveways are recessed from the face of the buildings and garages are tandem to minimize the visual impact of garage doors. These homes all include small private back yards.

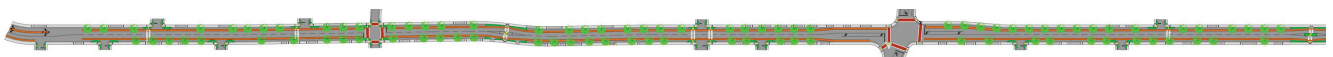
A small community open space is also provided at the center of the neighborhood as a place to gather, to pick up mail, and to also provide a location for some onsite water treatment and detention. This space would be privately maintained.



Existing aerial view



Conceptual Site Plan



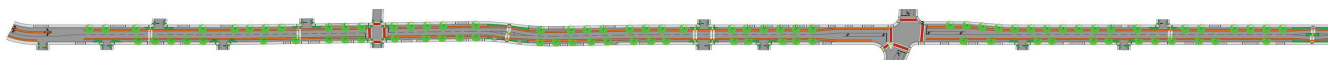
6. Library & Public Art: Celebrating the Neighborhood

Oak Park is a unique place with a rich history and a diversity of residents. This plan aims to celebrate the history that exists and to enhance the unique qualities of the place. It also aims to foster a stronger sense of community by improving the spaces where individuals interact. These public spaces include the community center, parks, schools, churches, community gardens, a future library, and even the sidewalk environment. The presence of art within these spaces can also create a sense of history, pride, and place.

Public Art can enhance the community by:

- Adding beauty
- Supporting local artists
- Creating Gateway Elements to celebrate arrival into the neighborhood
- Creating opportunities for schools and local youth to invest in their neighborhood
- Honoring the legacy of Dr. Martin Luther King, Jr.
- Telling a story of the history of the neighborhood.





Library

A new library facility rated very high in both community workshops and has strong community support. Oak Park is currently served by Colonial Heights Library. The service area is bounded generally by Highways 50 and 99, Power Inn Road, and Elder Creek. According to the 2007 Library Master Plan:

"The Colonial Heights area is expected to experience moderate growth through infill development over the next 20 years. However, the size of the existing library is too small to serve the community's residents. It is recommended that the current facility be both renovated and expanded on its current site to allow it to continue to serve the Fruitridge area of the City of Sacramento and the nearby County. There is not enough capacity at the current site, however, to expand the library to meet all of the area's library needs. A new (2015) facility in the area of 65th and Folsom Boulevard is recommended to serve both the northern part of the current Colonial Heights service area as well as eastern Sacramento City. An alternative development strategy is for a smaller addition to the Colonial Heights Library and larger library near 65th and Folsom Boulevard."

A focused service facility could be employed to fill an immediate need, develop a community partnership, and enhance customer service and convenience. These units could range from an automated reserve book kiosk, to a computer center in the community center, to a focused collection and reading room in a senior center. Electronic kiosks strategically located in shopping centers or community or civic centers would allow library customers to place a hold, pick up a reserved item, renew materials, or access the library's databases without traveling to the nearest branch library. These focused



Colonial Heights Library

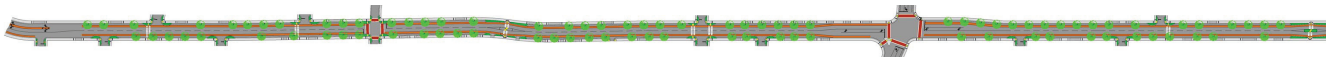
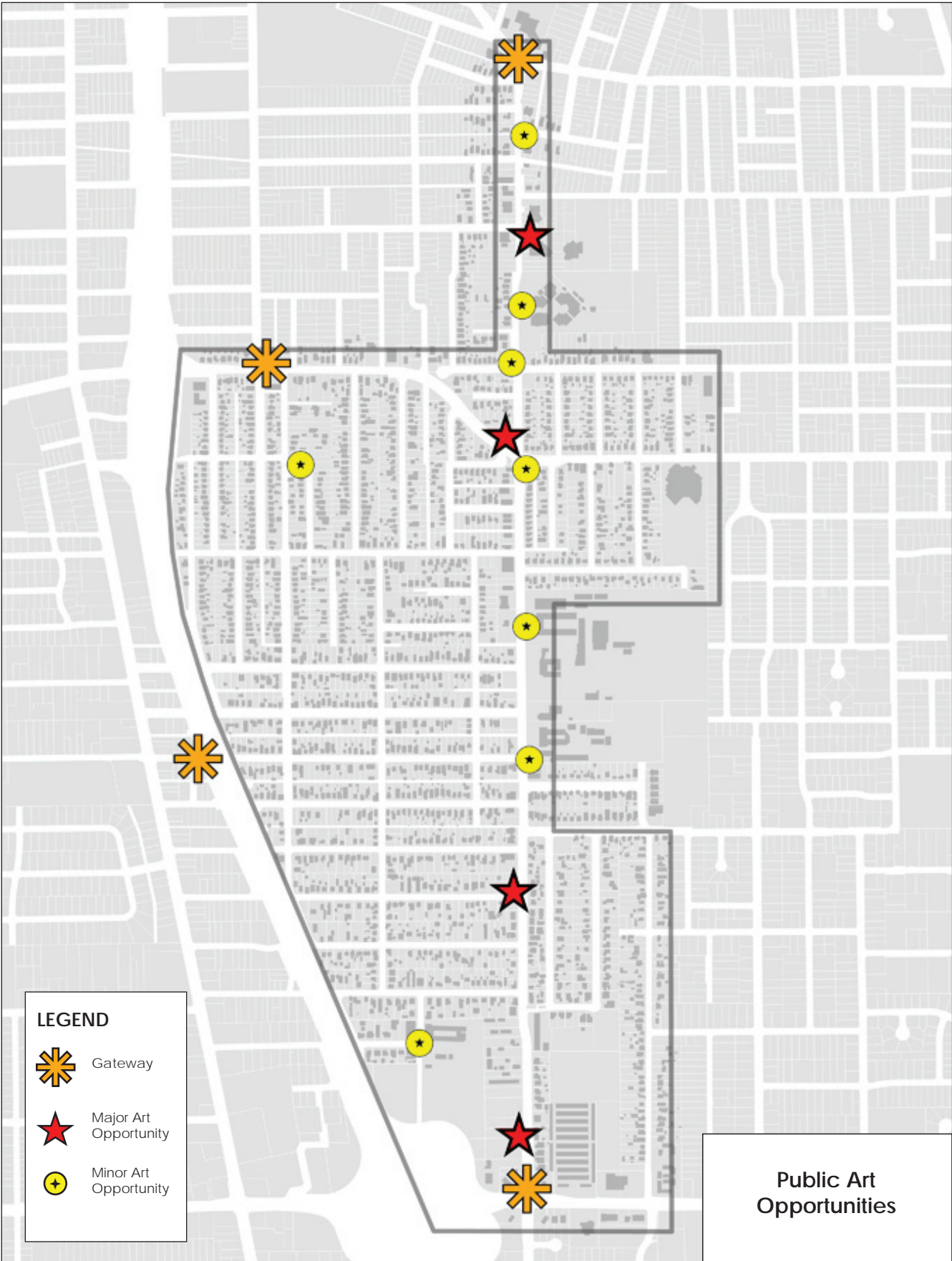
service units would quickly address a library service need in a cost-effective manner. The capital costs for these units would be significantly less than for a full-service branch library and the ongoing operating costs would largely depend on whether or not staff or programming was provided.

Focused-service facilities:

- Are designed to address a specific purpose in a specific setting for the people most likely to frequent that particular area
- Support a demonstrated customer need in the particular area
- Extend services beyond full-service branches rather than exist in place of a branch
- Provide sizes ranging from as small as 25 to 50 square feet to as much as 2,000 to 2,500 square feet, depending on the appropriate response to the defined need and to the circumstances of the facility location.

Currently the 2007 Library Master Plan does not identify a new full-service facility or a focused service facility in the greater Oak Park area. The Library Authority should be engaged to discuss these and other opportunities for services to the community.





Public Art Program

A public art program should be initiated before project design development and improvements are undertaken. Local artists should have many opportunities to create artful expressions that will enliven life along Martin Luther King, Jr. Boulevard.

An art program should be coordinated with the city's Art in Public Places program. Artists can be pre-qualified to submit proposals for public art in various locations along the boulevard - in major public spaces, in minor but meaningful locations, at community gateways, and in additional locations throughout the neighborhood. The art may vary by artist and by media. It may take the form of sculpture, mural, or landscape. A common theme may also unite different pieces into a cohesive whole.

Local schools should also be encouraged to participate in a public art program. The art of children has a special power to inspire hope and to create a sense of ownership and pride by the next generation living in the community.

Public Art Opportunities

As improvements are undertaken within the public sphere, the City and SHRA will allocate two percent of the construction costs toward an art in public places program. Pre-selected artists will design and create art pieces that can be located throughout the neighborhood. The adjacent map shows potential locations for 16 public art installations.

The public art is categorized here into 3 groups: Gateways, Major Art Opportunities, and Minor Art Opportunities. These three categories are described on the following pages.



Gateways

The West Gateway site has already been described in the Landscape Concepts chapter of this document. This location, along with the other three gateways shown on the Public Art Location Map, are all opportunities for Art that announces entry into South Oak Park.

Gateways can be elements that arch over streets; or landscape monuments along the side of the road or within medians; or they can integrate signage.



Arched gateway example



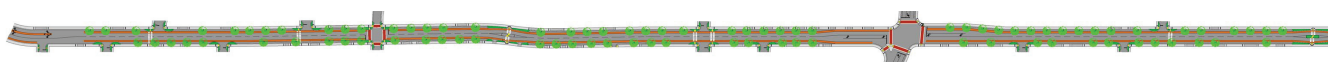
Arched gateway example



Pedestrian-scaled gateway art element



Median landscape monument



Major Art Opportunities

Major art opportunities are located in prominent public spaces such as parks or urban plazas. They can be landscape elements or sculptural elements. They can evoke serious historical memories or they can add fun and whimsy to a landscape.



Public art can entertain such as this piece in Melbourne



Memorial art can inspire



Public art in an urban plaza setting



Memorial and gathering space in Columbia, Missouri



Minor Art Opportunities

Art can also be found in unexpected places and this kind of art can also be meaningful. Simple art pieces along a sidewalk, for example, could tell a story that a child may read while walking to school. Minor art pieces are also opportunities for younger or less experienced artists to contribute their art to their communities.



Art embedded in sidewalks, Las Vegas, NV



Mosaic tile enlivens a pedestrian bridge in Austin, TX



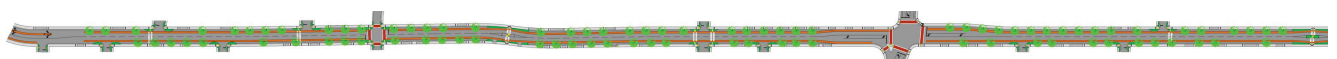
Sidewalk art can be ornate or simple; in grand public places as well as in unexpected places



Sidewalk art in Seattle



Colorful barriers enliven a sidewalk in Richmond, CA



7. Funding Resources

A number of funding sources could help implement plan recommendations. They offer alternatives for street design, community facilities, and other infrastructure.

The following funding sources are detailed here:

- City road maintenance and construction funds
- Development fees
- Special districts
- SACOG Community Design Grants
- Home Investment Partnership Program (HOME)
- RDA Housing Set-Aside
- California Trade and Commerce Agency
- Proposition 12 Tree Planting Grant Program
- Volunteer initiatives and private donations
- State and federal transportation funds
- Parks & In-lieu (Quimby) Fees



Each of these funding sources is subject to changes in state and federal law, budget levels, and target project priorities. A summary of the situation for each as it existed at the time of this writing follow below.

City Road Maintenance & Capital Funds

City road maintenance and construction funds can add striping, traffic calming, sidewalks, curbs and similar elements to other projects that already involve digging up or rebuilding street sections. For example, storm drain and sewer improvements, utility undergrounding projects, and routine street resurfacing are all projects that can provide an opportunity for implementing traffic calming measures.

The greater the extent of the reconstruction, the greater the opportunity for adding elements such as bulb-outs, mini-circles and raised intersections at a lower cost than that of a stand-alone project. Also, communities avoid the disruption, noise and expense of repeatedly digging up a street and detouring traffic.

Such combination projects will require coordination between departments, and capital improvement projects whose schedules and budgets are often separate.

Many cities have incorporated traffic calming into street reconstruction projects. In Venice, FL, for example, officials added \$80,000 to a previously planned Main Street resurfacing project to provide for intersection bulb-outs, mid-block bulb-outs, median crossings, and crosswalks of colorful paver stones.

Seattle has added planted medians to several streets at reduced cost as part of sewer upgrade projects. County transportation sales tax measures can provide substantial funding for city street maintenance and rehabilitation.

Development Fees

Some cities require developers to install or help pay for infrastructure improvements (streets, sidewalks, trails, landscaping, etc.) through individual development agreements. On a larger scale, SHRA could explore using development fees with a capital improvements program to help fund recommended improvements.

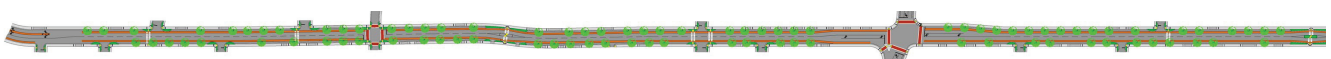
Special Districts

A special district such as a Business Improvement District (BID) can provide up-front and on-going funding for projects benefiting the area. Business-Based Improvement Districts are best suited for marketing, special events, and smaller expenditures like signage. Property-Based BIDs typically generate more revenues and are better suited for more expensive projects like landscaping. Landscaping and lighting districts can be established for streetscape improvements and maintenance.

Other types of facilities and infrastructure districts are sometimes created for parks, drainage and sewage. Special districts generally assess a charge levied upon parcels of real property within the district's boundaries to pay for "local improvements." To fund such a district it is necessary to charge an assessment or fee to property owners and/or merchants.

SACOG Community Design Grants

SACOG's Community Design Grants have been created to help implement their Blueprint Principles. The program is available for projects that are eligible for federal transportation funding. SACOG offers this program as funding becomes available, usually on a two year cycle. The program has three categories: Conventional, Complete Street, and \$100,000. By definition the improvements of the Martin Luther King Jr. Boulevard corridor fall into the Complete Street



category which is eligible for up to 7 million dollars per project. There is a required 12% match from local funds and stiff competition for the grant money. The program focuses on projects that are implementable in the 2 year funding cycle of the grant.

Home Investment Partnership Program (HOME)

The City is also an entitlement grantee for the receipt of HOME funds from HUD. The HOME program was created by the National Affordable Housing Act of 1990, and provides funds to participating jurisdictions for the following objectives: provide decent affordable housing to lower-income households; expand the capacity of nonprofit housing providers; strengthen the ability of state and local governments to provide housing; and leverage private sector participation in the provision of affordable housing. Funds may be used for a variety of affordable housing activities designed for income-eligible households including: home owner rehabilitation, homebuyer activities, rental housing and tenant based rental assistance. Funds must also be set aside for use by Community Housing Development Organizations (CHDOs) to carry out one of the eligible activities.

RDA Housing Set-Aside

Each Redevelopment Agency in California is required to set aside a minimum of 20 percent of its tax increment for the construction and/or rehabilitation of housing affordable to low-moderate income households. The RDA has established its Low-Moderate Income Housing Fund as the repository for its 20 percent set aside. Funds may be used for acquisition, new construction, rehabilitation, on-site facilities and related costs associated with the provision of affordable housing, including assistance with home ownership. Redevelopment agencies

must insure that a minimum of 15 percent of all housing units within a redevelopment project area are affordable to low-moderate income households, with no less than 6 percent of all housing units affordable to very-low income households.

Proposition 12 Tree-Planting Grant Program

This California Department of Urban Forestry program provides over \$1 million per year in grants to cities, counties, districts and nonprofit organizations for planting, and three years of maintenance of trees in urban public settings.

The maximum award is \$25,000 for a "small population community" and \$50,000 for "regular Proposition 12 applicants." For more information: www.ufe.org/files/grantinfo/Prop12Planting-Grants.html. For other possible funding sources for downtown trees: www.californiareleaf.org/grants_guide.html.

Volunteer Initiatives and Private Donations

In addition to funding sources, programs can be created for volunteer initiatives such as "Adopt-a" programs where individuals or groups engage in beautification projects such as tree plantings. A program can also fund some projects, such as public art, by enlisting private donors to sponsor downtown enhancement activities. These programs can be administered by the City or by other community organizations.

State and Federal Transportation Funds

Major state and federal transportation funding resources are outlined below. For more information on these funding programs, visit Caltrans' Division of Local Assistance website: www.dot.ca.gov/hq/LocalPrograms



State Transportation Improvement Program (STIP)

Funded at \$8.3 billion over 1999-2005, this program represents the lion's share of California's state and federal transportation dollars. Three-quarters of the program's funds were earmarked for improvements determined by locally adopted priorities contained in Regional Transportation Improvement Programs (RTIP), submitted by regional transportation planning agencies from around the state

STIP funds can be used for a wide variety of projects, including road rehabilitation, road capacity, intersections, bicycle and pedestrian facilities, public transit, passenger rail and other projects that enhance the region's transportation infrastructure.

The 2004 STIP was adopted by the California Transportation Commission, the body that ultimately programs projects by adopting the STIP, on August 5, 2004.

Transportation Enhancement Funds

Federal Transportation Enhancement funds are for construction projects that are "over and above" normal types of transportation projects. These projects may include street trees and landscaping along roadways, pedestrian and bicycle access improvements and other scenic beautification. These are apportioned throughout the county.

California Department of Transportation Planning Grants

The California Department of Transportation (Caltrans) provides one-time transportation planning grants of up to \$300,000 for environmental justice context-sensitive planning. The environmental justice grants are intended to promote public participation and context-

sensitive planning to improve mobility, access, equity and quality of life for underserved communities. A local contribution of 10 percent is required, with in-kind contributions limited to 5 percent. For more information visit the California Department of Transportation's Transportation Planning Grants website.

Hazard Elimination Safety Program (HES)

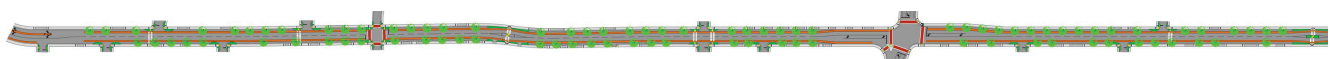
The Hazard Elimination Safety Program is a federal safety program that provides funds for safety improvements on all public roads and highways. These funds serve to eliminate or reduce the number and/or severity of traffic accidents at locations selected for improvement. Some of the street design elements recommended may be eligible for funding if the site selected is considered a high hazard location. Caltrans solicits applications for projects. Any local agency may apply for these safety funds.

Safe Routes to School (Caltrans)

Caltrans administers state and federally funded programs to improve walking and bicycling conditions to and from schools. Projects for federal funding must fall under infrastructure (capital) or non-infrastructure (education and encouragement) categories.

A standardized statewide SRTS training program with promotional materials and school resources will be developed to help communities implement programs.

The program seeks to fund projects that incorporate engineering, education, enforcement, encouragement and evaluation components. For more information: www.dot.ca.gov/hq/LocalPrograms/saferoute2.htm.



Bicycle Transportation Account (BTA)

This state fund, administered by the Caltrans Bicycle Facilities Unit, can be used to aid cyclists, including median crossings, bicycle/pedestrian signals and bike lanes. After 2005-06, annual BTA funding will be \$5 million.

To be eligible for BTA funds, a city or county must prepare and adopt a Bicycle Transportation Plan. Adoption of a plan establishes eligibility for five consecutive funding cycles.

Transportation Development Act (TDA)

TDA provides for two sources of funding: Local Transportation Funds (LTF) and State Transit Assistance (STA). The TDA funds a wide variety of transportation programs, including planning and program activities, pedestrian and bicycle facilities, community transit services, public transportation, and bus and rail projects.

The STA fund can only be used for transportation planning and mass transportation purposes. Annual apportionments of TDA funds for region are distributed to transportation projects by the Sacramento Area Council of Governments.

Parks Dedication / In Lieu (Quimby) Fees

Cities and counties have been authorized since the passage of the 1975 Quimby Act (California Government Code §66477) to pass ordinances requiring that developers set aside land, donate conservation easements, or pay fees for park improvements. Revenues generated through the Quimby Act cannot be used for the operation and maintenance of park facilities.

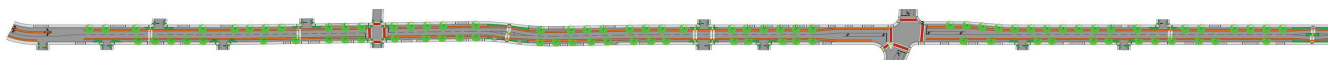
The goal of the Quimby Act was to require developers to help mitigate the impacts of property improvements. The act gives authority for passage of land dedication ordinances only

to cities and counties. Special districts must work with cities, and/or counties to receive parkland dedication and/or in-lieu fees. The fees must be paid and land conveyed directly to the local public agencies that provide park and recreation services community-wide.

In Sacramento they are used for acquisition, improvement, and expansion of park, playground and recreational facilities or the development of public school grounds. Land dedication, or a combination of fee and land dedication may be required. The number of acres and/or dollar amount is calculated by either the appraised value of the land, or by a set amount associated with the planning area within which the development is located. When land is dedicated, the developer is required to complete public improvements. When a fee is paid in lieu of land dedication, an additional 20% is charged to cover public improvements. In lieu fees must be spent within the planning area where they are collected.

For more information go to: <http://maps.cityofsacramento.org/feetool/one-deets-std.jsp?id=217>





8. Next Steps

This Master Plan is a living document that represents the community's vision at this stage of revitalization. Many steps must now be taken in order to implement the vision. These steps are anticipated to take a number of years to realize. Below are some initial assumptions about the necessary actions to realize the plan.

Next Steps

- Funding Authorization
- Initiate CEQA
- Design Development Phase I
 - ◊ Surveys
 - ◊ Coordinate with SMUD
 - ◊ Coordinate with RT
 - ◊ Coordinate with Sacramento Metropolitan Arts Commission
 - ◊ Coordinate with Wet and Dry Utilities
- Initiate Design
- Approval of Design Development documents
- Prepare Contract Documents
- Bidding

Budget Summary

MLK Streetscape Improvements	\$8,133,000
21st Avenue Improvements	\$294,000
Memorial Park (12th Avenue & MLK)*	\$842,000
24th Avenue @ MLK Park*	\$490,000
Temple Park Improvements*	\$300,000
Community Center Plaza & Street Realignment	\$1,116,000
21st Avenue Gateway	\$56,000
12th Avenue Gateway*	\$168,000
Subtotal	\$11,099,000
Art in Public Places @ 2%	\$228,000
Total	\$11,627,000

* Does not include land acquisition



Phasing

Phase I

- MLK from Broadway to 12th including Community Center excluding 12th Avenue intersection, Memorial Park and landscaping
- 21st Avenue Bulb-outs, Crosswalk Enhancements and Acorn Streetlights
- 21st Avenue Gateway concurrent with work on Franklin mini-park
- Gateway signage at Fruitridge
- Alley assessment working with neighbors
- Negotiate with property owners for the acquisition of key opportunity sites
- Negotiate with property owners for the acquisition of the 24th Avenue Park site
- Negotiate with property owners for the acquisition of the 12th Avenue Gateway site

Phase I Budget Estimate

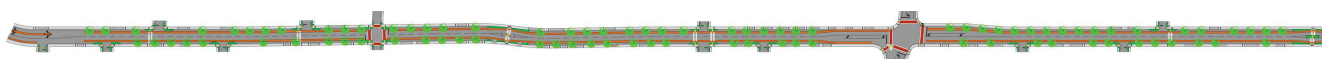
Project	Cost
Broadway to 12/14th Avenue Improvements	\$2,019,000
Community Center Plaza & Street Realignment	\$1,116,000
SMUD Underground	\$750,000
21 Avenue Improvements	\$294,000
21st Avenue Gateway	\$56,000
Subtotal	\$4,235,000
Art in Public Places	\$84,700
Total	\$4,319,700

Phase II

- MLK from 12th Avenue to just south of 21st Avenue including 12th Avenue intersection and landscaping

Phase II Budget Estimate

Project	Cost
12/14th Avenue to 21st Avenue Improvements	\$1,605,000
SMUD Underground	\$750,000
Subtotal	\$2,355,000
Art in Public Places	\$47,100
Total	\$2,402,100



Phase III

- MLK from 21st Avenue to Fruitridge

Phase III Budget Estimate

Project	Cost
21st Avenue to Fruitridge Improvements	\$2,259,000
SMUD Underground	\$750,000
Subtotal	\$3,009,000
Art in Public Places	\$60,200
Total	\$3,069,200

Phase IV

- 24th Avenue Park
- Improvements to Temple Park
- 12th Avenue Gateway
- Memorial Park

Phase IV Budget Estimate

Project	Cost
24th Avenue Park	\$490,000
12th Avenue Gateway	\$168,000
Memorial Park	\$842,000
Temple Park Improvements *	\$300,000
Subtotal	\$1,800,000
Art in Public Places	\$36,000
Total	\$1,836,000

* Includes cost of rehabilitating existing park only, not land acquisition or street improvements

